

## **Current and Proposed Evergreen NASP Changes**

This document contains a detailed breakdown of how the proposed Evergreen Neighbourhood Area Structure Plan (NASP) will differentiate from the existing Evergreen NASP. This cover page is provided as a summary of the changes to help readers get a sense of what these detailed changes affect in a more concise format.

### Changes in the Proposed NASP:

- Redesigning the Eastern half housing layout and road network.
- Change of Reverse Housing area to standard oriented lots.
- Changing the central commercial parcel to high-density residential.
- Realigning neighbourhood node position to focus on community park and recreation area.
- Providing an anticipated additional 199 dwelling and 420 people to bring the neighbourhood total to approximately 1,101 dwellings and 2,636 residents.
- Making corresponding updates to data tables and figures
- Assorted language cleanups throughout to align the Evergreen NASP with the new Zoning Bylaw regulations approved in 2024.
- General text formatting and layout cleanups.

For more details, please contact Christi Fidek, Senior Planner at [christi.fidek@reddeer.ca](mailto:christi.fidek@reddeer.ca).

## Evergreen NASP 2025 Amendment Comparison

Text changes identified in **red** font

- New text – *italics*
- Deleted text – ~~strike through~~
- Information added to explain change - *\*starred text\**

Current NASP	Proposed NASP
<p>Page (i)</p> <p><b>Evergreen's Vision</b></p> <p>The Evergreen neighbourhood will be a vibrant residential community in northeast Red Deer that is home to approximately 2,216 residents. Evergreen will respect the unique natural features in an effort to protect wildlife habitats, enabling residents and visitors to enjoy beautiful natural surroundings. As a result of its unique open space features, access to primary roadways, and proximity to nearby amenities; Evergreen will be one of the most desirable residential areas in the entire City.</p>	<p>Page (i)</p> <p><b>Evergreen's Vision</b></p> <p>The Evergreen neighbourhood will be a vibrant residential community in northeast Red Deer that is home to approximately <b>2,636</b> <del>2,216</del> residents. Evergreen will respect the unique natural features in an effort to protect wildlife habitats, enabling residents and visitors to enjoy beautiful natural surroundings. As a result of its unique open space features, access to primary roadways, and proximity to nearby amenities; Evergreen will be one of the most desirable residential areas in the entire City.</p>
<p>Page (ii)</p> <p><b>AMENDMENT 2019</b></p> <p>The Evergreen NASP was originally approved by the City of Red Deer in September of 2014. An amendment was approved in March of 2019 to facilitate the following two changes:</p>	<p>Page (ii)</p> <p><b>AMENDMENT</b></p> <p><b>2019</b></p> <p>The Evergreen NASP was originally approved by the City of Red Deer in September of 2014. An amendment was approved in March of 2019 to facilitate the following two changes:</p>

1. Redesign of the one-way roadway in the southern portion of the neighbourhood to alleviate challenges related to waste management.
2. Convert a portion of R1 Low Density Residential Lots in the southwest to R1G Small Lot Residential, in response to current market conditions.
3. Additional of a 0.04 ha parcel of municipal reserve south of Elder Close to connect to the Emerson neighbourhood.
4. Addition of two trail crossings across the south legs of the natural area to facilitate better trail connections.

### **Redesign of One-Way Roadway**

As described in the 2014 Evergreen NASP, a 13.3m One-Way Local Roadway was proposed along the south boundary of the Plan Area (Elder Close). Following approval, it was determined by the City of Red Deer that the one-way roadway design would not function once the City transitions to automated waste collection. Collection trucks would not be able to collect waste from the interior homes. To facilitate waste collection, the Developer was asked to redesign the south p-loop accordingly.

### **Increase the Amount of R1G**

At the time of Evergreen's approval, the R1G Small Lot Residential land use was new for the City of Red Deer and had not yet been widely constructed or accepted; as such, the use of this land use in Evergreen was limited. Since approval, many R1G homes have been constructed in Vanier East and Laredo and have been quickly accepted as a housing standard.

The R1G land use district is a tool to assist with increasing the City's density, lower infrastructure costs, be competitive with surrounding

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The R1G land use district is a tool to assist with increasing the City's density, lower infrastructure costs, be competitive with surrounding

communities thereby retaining and attracting new residents and address residential affordability. This similar land use district exists in Calgary, Edmonton, Airdrie, and Sylvan Lake to name a few.

#### **AMENDMENT 2021**

A 2021 NASP amendment was completed to amend the land use of one parcel of land. Shifting housing trends and market demands warranted:

1. Change one R3 site to R2T and R1A.
2. Change Community Amenity sites to alternate approved zoning.

#### **Change land use on R3 site**

A 0.96 ha R3 site was originally identified along Evergreen Way and intended to be built as a multi-storey apartment complex. The proposed amendment maintains medium and high density residential via fee simple dwellings duplex and townhomes. This revision allowed for continuation of townhomes along the entrance road and a parcel of R1A along the north of the parcel.

#### **Change Community Amenity sites**

Evergreen originally proposed two Community Amenity sites. Both sites had alternate land uses in the event that they were not purchased and developed for community amenities. Both sites were advertised according to the City of Red Deer's guidelines, due to a lack of interest both sites were rezoned to their alternate land uses on May 1, 2017.

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**2025**



	<p><i>In 2025, a comprehensive review of the undeveloped area was completed to improve the layout and better reflect market trends, desired housing types, and land uses.</i></p> <p><i>The amendment also updates all land use districts as per the 2024 Zoning Bylaw.</i></p>
<p>Page (iii)</p> <p><b>Relevant Planning Documents</b></p> <p>The Evergreen NASP has been created to function with and respect existing planning documents. The following relevant documents have been reviewed and referenced in preparation of this NASP:</p> <ul style="list-style-type: none"> <li>• Province of Alberta - Municipal Government Act (2000)</li> <li>• Stantec Consulting Ltd – Northland Drive/20 Avenue Functional Planning Study (2008)</li> <li>• The City of Red Deer – 2004 Growth Study (2005)</li> <li>• The City of Red Deer - 2012/2014 Strategic Direction (2011)</li> <li>• The City of Red Deer – Commercial Opportunities Study (2010)</li> <li>• The City of Red Deer – East Hill Major Area Structure Plan (2005/2012)</li> <li>• The City of Red Deer – Intermunicipal Development Plan (2007)</li> <li>• The City of Red Deer – Land Use Bylaw (2006)</li> <li>• The City of Red Deer - Mobility Playbook (2013)</li> <li>• The City of Red Deer – Municipal Development Plan (2008)</li> <li>• The City of Red Deer – Neighbourhood Planning and Design Standards (2013)</li> <li>• The City of Red Deer - River Valley and Tributaries Park Concept Plan (2010)</li> <li>• The City of Red Deer - Section 26 Multi-Neighbourhood Plan (2013)</li> <li>• The City of Red Deer – Trails Master Plan (2005)</li> </ul>	<p>Page (iii)</p> <p><b>Relevant Planning Documents</b></p> <p>The Evergreen NASP has been created to function with and respect existing planning documents. The following relevant documents have been reviewed and referenced in preparation of this NASP:</p> <ul style="list-style-type: none"> <li>• Province of Alberta - Municipal Government Act (2000)</li> <li>• Stantec Consulting Ltd – Northland Drive/20 Avenue Functional Planning Study (2008)</li> <li>• The City of Red Deer – 2004 Growth Study (2005)</li> <li>• The City of Red Deer - <del>2012/2014</del> <b>2023-2026</b> Strategic Direction <del>(2011)</del> <b>(2023)</b></li> <li>• The City of Red Deer – Commercial Opportunities Study (2010)</li> <li>• The City of Red Deer – East Hill Major Area Structure Plan <del>(2005/2012)</del> <b>(2021)</b></li> <li>• The City of Red Deer – Intermunicipal Development Plan (2007)</li> <li>• The City of Red Deer – <del>Land Use</del> <b>Zoning</b> Bylaw <del>(2006)</del> <b>(2024)</b></li> <li>• The City of Red Deer - Mobility Playbook (2013)</li> <li>• The City of Red Deer – Municipal Development Plan <del>(2008)</del> <b>(2013)</b></li> <li>• The City of Red Deer – Neighbourhood Planning and Design Standards <del>(2013)</del> <b>(2022)</b></li> <li>• The City of Red Deer - River Valley and Tributaries Park Concept Plan (2010)</li> <li>• The City of Red Deer - Section 26 Multi-Neighbourhood Plan (2013)</li> <li>• The City of Red Deer – Trails Master Plan (2005)</li> </ul>

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*The City of Red Deer's Strategic Direction* guides the City along a path and provides focus and purpose. It is the City's most important plan as it shapes the organization, the municipal programs, and services they provide. In 2011, the strategic direction for 2012-2014 was created which strived to make Red Deer a healthy community.

Page (v)

## 2004 Growth Study (2004)

The purpose of the *2004 Growth Study* was to focus on land absorption rates and suture land inventory requirements for industrial, residential, and commercial land uses within the City of the following 50 years. The Study was also to consider future need to open space, environmental preservation areas, and public service uses. In this Study, the Evergreen Plan Area was identified in Growth Sector B, for future residential development at the population threshold of 90,000-115,000.

## Municipal Development Plan (2008)

*The City of Red Deer Municipal Development Plan (MDP)* outlines broad policies for guiding growth and changes in the City for the next twenty-five years. Among many other things, the MDP sets out the following policies regarding neighbourhood designs:

- Density in new neighbourhoods shall ensure a minimum of 14.80 dwelling units per net developable hectare.
- The City shall continue to require a mix of housing types and forms in all residential neighbourhoods.

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The Evergreen Plan Area is identified in the MDP for future residential development; there are no constraints listed for the development.

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### East Hill Major Area Structure Plan (2013)

*The City of Red Deer East Hill Major Area Structure Plan (MASP) sets out the broader transportation and land use objectives for multiple quarter sections in east Red Deer.*

The following elements were shown within the MASP which may affect the planning of the Evergreen Lands Plan Area.

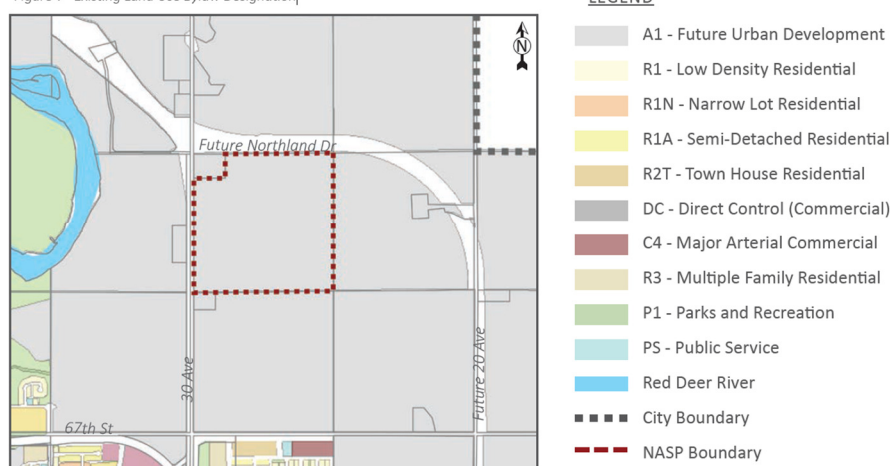
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### Neighbourhood Planning and Design Standards (2013)

*The City of Red Deer's Neighbourhood Planning Design Standards states the following matters must be considered when preparing a Neighbourhood Area Structure Plan:*

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Figure v - Existing Land Use Bylaw Designation



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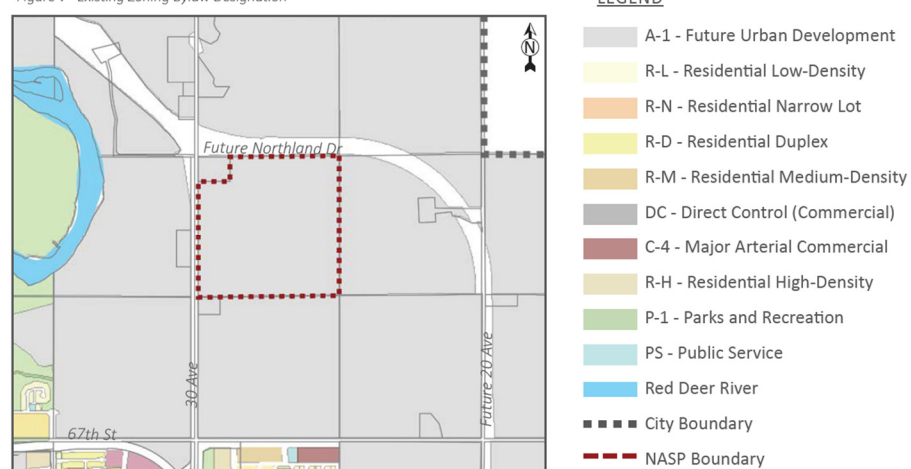
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### Land Use Bylaw (2006, 2013)

The City of Red Deer Land Use Bylaw (LUB) describes all available land use districts to be utilized throughout the City and identifies any potential land use constraints available at the time of its creation. There are no constraints listed for the development of the Plan Area in LUB. This Plan has been developed to conform to the bylaw and all its land use regulations.

Subsequent to NASP approval, the Land Use Map will be amended to redesignate the Evergreen Plan Area to the land uses as described in this Plan.

**On behalf of Melcor Developments Ltd., Stantec Consulting Ltd. has prepared this NASP; which meets the requirements as previously identified.**

Page 7

### DEMOGRAPHICS

The following information is based on the City of Red Deer's 2013 Municipal Census, and 2011 Statistics Canada Information.

The demographics of Red Deer demonstrate that Red Deerians are generally younger, with smaller families, and a higher income than average Canadians. This information can be utilized to determine what types of land uses or housing types would be best suited in Evergreen.

### Population

In 2013 the City of Red Deer was the third largest city in Alberta with a population of 97,109. Located directly between Calgary and

### ~~Land Use Zoning Bylaw (2006, 2013)~~ (2024)

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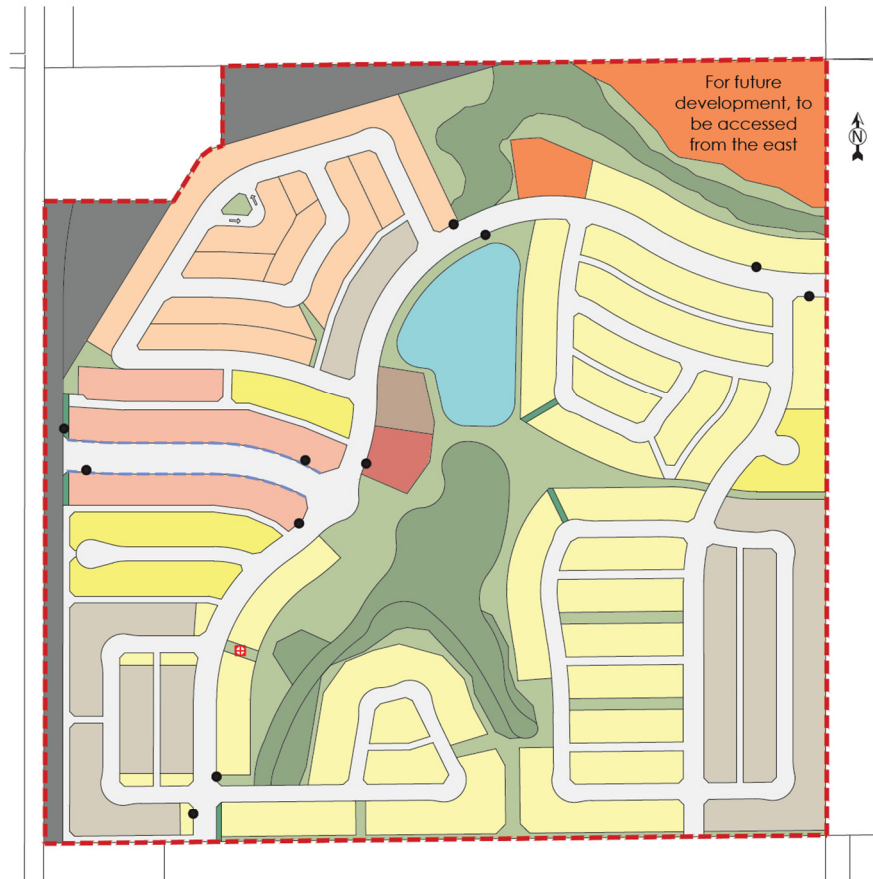
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<p>Edmonton, Red Deer is at the center of the Calgary/Edmonton Corridor, one of the fastest growing regions in Canada. Red Deer's population has steadily increased at a compound growth rate of 2.94% over the past 10 years, from 72,691 persons in 2003. According to Statistics Canada, the two main factors of growth in this area are: migration from other provinces, and natural increase.</p> <p>The City's population was recorded as being 49.4% female and 50.1% male with an average age of 32 and a mode age (age most frequently recorded) of 25; lower than the provincial average of 36 and national average of 40. Red Deer's younger population may result in more single residents or young families.</p> <p><b>Income</b> In 2010 Red Deer's families experienced a median after-tax income that was almost \$10,000 above the Canadian average. In addition, the percentage of after-tax low income families was 11.6% in Red Deer compared to 14.9% Canada-wide.</p>	<p>Calgary and Edmonton, Red Deer is at the center of the Calgary/Edmonton Corridor, one of the fastest growing regions in Canada. Red Deer's population has steadily increased at a compound growth rate of <del>2.94%</del> 0.4% over the past <del>10</del> 5 years, from <del>72,691</del> 100,418 persons in <del>2003</del> 2016. According to Statistics Canada, the two main factors of growth in this area are: migration from other provinces, and natural increase.</p> <p>The City's population was recorded as being 49.4% female and <del>50.1%</del> 50.6% male with an average age of <del>32</del> 39 and a mode age (age most frequently recorded) of <del>25</del> 36; lower than the provincial average of <del>36</del> 38 and national average of 40. Red Deer's younger population may result in more single residents or young families.</p> <p><b>Income</b> In <del>2010</del> 2020 Red Deer's families experienced a median after-tax income that was almost <del>\$10,000</del> \$2,800 above the Canadian average. <del>In addition, the percentage of after-tax low income families was 11.6% in Red Deer compared to 14.9% Canada-wide.</del></p>
<p>Page 10</p> <p><b>Population</b> Approximately 2,161 residents will call Evergreen home, living in a mix of housing types comprised of 902 dwelling units. The variety of housing types in Evergreen will allow young couples to find starter homes, college students to find affordable rental units, professionals to grow their families, and empty-nesters to retire comfortably.</p>	<p>Page 10</p> <p><b>Population</b> Approximately <del>2,161</del> 2,600 residents will call Evergreen home, living in a mix of housing types comprised of <del>902</del> 1,101 dwelling units. The variety of housing types in Evergreen will allow young couples to find starter homes, college students to find affordable rental units, professionals to grow their families, and empty-nesters to retire comfortably.</p>
<p>Page 11</p>	<p>Page 11</p> <p><i>*updated figure and legend to align with new proposed layout*</i></p>

Figure 7 - Land Use Concept Plan



**Legend**

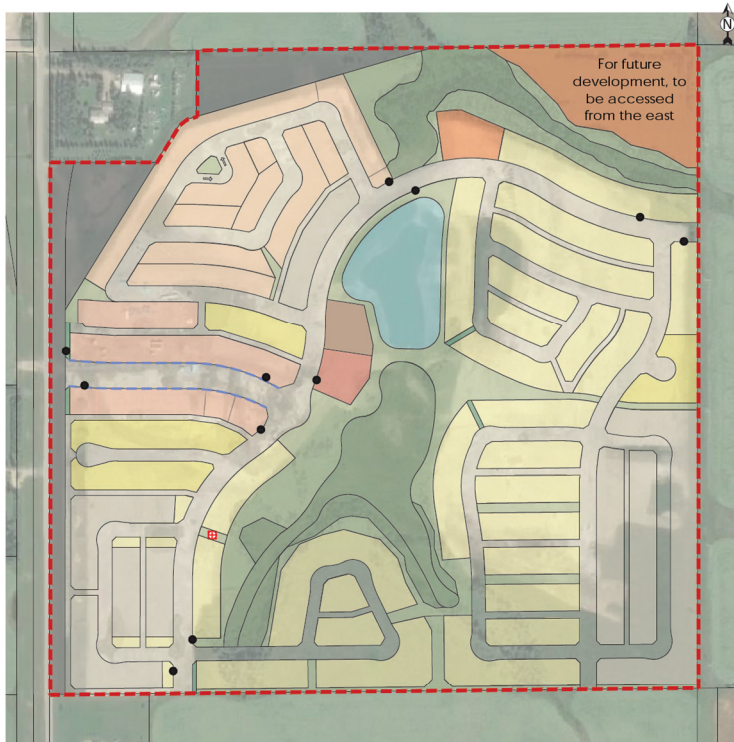
R1 Low Density	C3 Neighbourhood Convenience	Protected Parking Area
R1WS Wide Shallow	Municipal Reserve (MR)	Road Widening
R1G Small Lot	Environmental Reserve (ER)	NASP Boundary
R1A Semi-Detached	Public Utility Lot (PUL)	
R2T Town House	Stormwater Management (PUL)	
R2 Medium Density	Well Site	
R3 Multiple Family	Potential Transit Stop	



Figure 7 - Land Use Concept Plan



Figure 8 - Land Use Concept Plan over Aerial

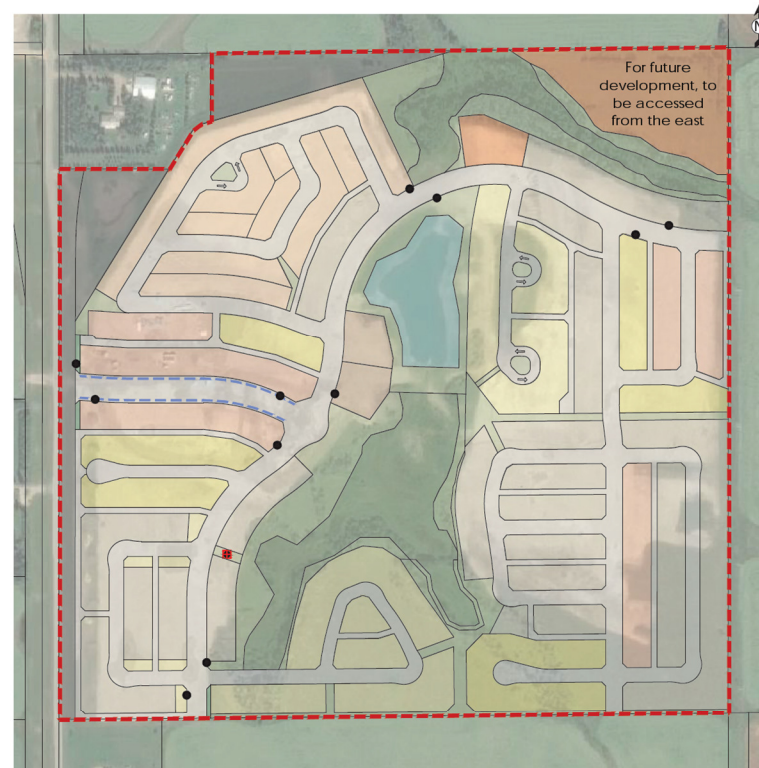


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Figure 8 - Land Use Concept Plan over Aerial



**Legend**

R-L Residential Low Density	Municipal Reserve (MR)	Well Site
R-W Residential Wide	Environmental Reserve (ER)	Potential Transit Stop
R-N Residential Narrow Lot	Public Utility Lot (PUL)	Protected Parking Area
R-D Residential Duplex	Stormwater Management (PUL)	Road Widening
R-M Residential Medium-Density		NASP Boundary
R-M Residential Medium-Density (Town House)		
R-H Residential High-Density		



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Evergreen's winter city design is discussed further on page 54.

Page 18 - 26

## Overview

The Evergreen neighbourhood has been designed to create highly desirable properties for residents of all age, lifestyle, and income. By providing a variety of lot sizes and housing types, a range of residents will be able to call Evergreen home.

Table 1 - Housing Mix.

Housing Mix	Hectares	Acres	# of Dwelling Units
<b>Total Housing Stock</b>	<b>31.58</b>	<b>78.03</b>	<b>902</b>
R1 Residential (Low Density) District	12.80	31.64	294
*R1 Secondary Suites (15% of R1 units)	--	--	44
R1G Residential (Small Lot) District	7.00	17.29	177
R1WS Residential (Wide/Shallow Lot) District	4.06	10.04	112
R1A Residential (Semi-Detached Dwelling)	2.27	5.61	65
R2 Residential (Medium Density) District	2.79	6.89	101
R2T Residential (Town House) District	2.31	5.70	80
R3 Residential (Multiple Family) District	0.34	0.85	29

## Types of Housing

### Single Family

#### R1 Single Detached

Though the majority of lots within Evergreen have been zoned using the R1 Residential Low Density District, housing types in these areas may vary from standard single family homes to those listed below. The location of these particular housing types shown on **Figure 9 - Residential Concept Plan**.

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Evergreen's winter city design is discussed further on page 54 47.

Page 18 -20

*\*Pages adjusted, reorganized and condensed to be fewer overall pages with changes outlined below\**

*\*Table 1 Removed\**

*\*Text on pages removed and replaced with the following\**

## Overview

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## Types of Housing

### Single Family

*Although the majority of lots within Evergreen are anticipated to be single family, a variety of housing styles and price points is supported. Single family areas will include Residential Low Density Zone (R-L), Residential Narrow Lot Zone (R-N), and Residential Wide Zone (R-W). The location of these particular zones are shown on Figure 9 - Residential Concept Plan.*

## APPROPRIATE HOUSING STYLES

*Standard*

## APPROPRIATE HOUSING STYLES

### *Standard*

The assumed average size of a single detached home lot in Evergreen is 464.0m<sup>2</sup>; although, the minimum lot area is 360.0m<sup>2</sup> with a minimum width of 12.0m and depth of 30.0m respectively. The majority of these homes will be constructed with front attached garages.

Design of standard single family homes should consider a high level of architectural detail in particular to minimize the impact of the front garage.

### *Reverse Housing*

Typically homes are designed with their primary entrance off the front public roadway and secondary entrance from the rear of the property. The front door typically enters into an entryway whereas the rear door enters directly into a living space such as a dining area, living room, or kitchen. Unlike typical homes, reverse housing is just that - the primary entrance into the home faces the rear property and is designed to enter into an entryway or boot room rather than living space. The secondary entrance into the house is then off of the public road or lane, which is also designed to include a small entryway.

This type of design is commonly used in cottage homes where the home highlights views and access to the area behind the house, away from the roadway. To facilitate this reverse home style, houses are constructed with dual facades of equal architectural design: one facing the front street, and one facing the rear property.

Reverse housing has been located in areas with open space between the homes to create a public park space that will extend the outdoor

*Design of standard single family homes should consider a high level of architectural detail in particular to minimize the impact of the front garage.*

### **Walk-Out Basements**

*Homes located on slopes or along open spaces are often designed with walk-out basements. These type of homes will be designed with a deck off the main floor, and a door to the backyard from the basement. Walk out basements appeal to a variety of residents for different reasons including: an increased amount of natural light in basements making the space more livable, the ability to have bedrooms in the basement with full size windows, and increased air quality/reduced moisture in the basement.*

### **Additional Suites**

*Additional suites will be accommodated as per the City of Red Deer Zoning Bylaw which states that the maximum for any neighbourhood is 15%. Homes with additional suites will require additional parking as per the City's ZB.*

### **HEIGHT**

*The maximum height for single family homes in these districts will be 3 storeys or 15m measured from the average lot grade.*

### **PARKING**

*The majority of parking in the R-L, R-N and R-W districts will be via front attached garage and driveway. As per the City's Zoning Bylaw, two on-lot parking spaces will be provided per home. Additional parking will be available on-street.*

### **ACCESS**

amenity area of each home by visually connecting their lots to public green space. This style of housing appeals to a variety of residents including those who enjoy the immediate physical and visual access to the open space along their property.

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### **Secondary Suites**

Secondary suites are self-contained dwelling units located inside single-detached dwellings. These suites have a separate entrance either from a common area or outside. Secondary suites are an increasingly popular and make housing more affordable both for the renter and the home owner.

### **DENSITY**

The maximum density in this district is 27.7 du/ha; however, it is estimated that density will be approximately 23.0 du/ha. This assumption is based on an assumed average lot size of 435.2m<sup>2</sup>.

### **HEIGHT**

The maximum height for homes in this district will be 2 storeys or 10m measured from the average lot grade.

*Access to single family homes will be via the front street, with some lots accessible via the rear lane; this will also allow for additional rear parking or storage of recreation vehicles.*

### **Multi-Family**

*Two types of multi-family housing will be available within Evergreen, Residential Duplex Zone (R-D) and Residential Medium-Density Zone (R-M). These zones will contain duplex and townhouse dwellings and can be situated of fee simple lots or as part of a condominium development.*

### **APPROPRIATE HOUSING TYPES**

*Appropriate housing types for this district include bungalow, bi-level, or two-storey duplex and townhome housing. These units may or may not include front garages.*

### **ADDITIONAL SUITES**

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### **HEIGHT**

*The maximum height for homes in this district will be 3 storeys or a 15m height measured from the average lot grade.*

### **PARKING**

*Parking in R-D and R-M district will be via front attached garage and driveway or rear lane based on housing style and desired price point. As per the City's zoning, two parking spaces will be provided per home.*

## PARKING

The majority of parking in the R1 district will be via front attached garage and driveway. As per the City's LUB, two on-lot parking spaces will be provided per home. Additional parking will be available on-street.

## ACCESS

Access to R1 homes will be via the front street. Some R1 lots will also be accessible via the rear lane for municipal servicing purposes; this will also allow for additional rear parking or storage of recreation vehicles.

## SECONDARY SUITES

Secondary suites will be accommodated as per the City of Red Deer Land Use Bylaw which states that the maximum for any neighbourhood is 15%. Homes with secondary suites will require additional parking as per the City's LUB. For this reason, secondary suites are not envisioned in reverse housing as it may not be possible to meet additional parking requirements in these areas.

## R1WS Wide-Shallow Homes

Wide-shallow lots provide a similar home to a wide R1 lot, with a shallower yard. The increased width of the home itself minimizes the prominence and appearance of the garage while allowing more interaction between the home and the streetscape via windows, porch areas, etc. This increased frontage also provides for a more diverse and appealing streetscape. The wide-shallow housing style is more affordable than an R1 lot by offering a reduced lots size while still including a single or double-car attached garage.

## ACCESS

*Access to R-D and R-M homes will be via the front street or rear lane which will provide opportunities for front attached garage or rear pads which will also allow for additional rear parking or storage of recreation vehicles.*

## R-H Multiple Family

*R-H Multiple Family areas can take a variety of forms as further described below. This type of housing provides a more affordable residential option that appeals to a variety of residents including but not limited to renters, first time home buyers, and retirees. One R-H area has been located in Evergreen at the terminus of the gateway roadway as part of the community node. This location was chosen based on the anticipated location of public transit stops and proximity to the open space network and community amenities.*

## APPROPRIATE HOUSING TYPES

*This R-H multi-unit housing will be constructed based on market conditions at the time of development and could include one of the following. All building types listed below can take the form of rental or condo facilities. Regardless of building type, this site should be designed as a focal feature of the community with a high level of architectural detail and visual interaction at street-level.*

## Apartment-Style

*Apartment style buildings have shared entries, hallways, and often building amenities such as fitness centres or hot tubs.*

## Multiplex Building

The City of Red Deer's Land Use Bylaw contains several building design regulations which must be followed for building these types of homes. Regulations include those addressing the garage size, driveway width, windows along the front street, use of porch, and other design features such as gables.

### **APPROPRIATE HOUSING TYPES**

Housing types in this district could include bungalow, bi-level, or two-storey homes.

### **DENSITY**

The maximum density in this district is 34.7 du/ha; however, it is estimated that density will be approximately 27.6 du/ha. This assumption is based on an assumed average lot size of 362.1m<sup>2</sup>.

### **HEIGHT**

The maximum height for homes in this district will be 2.5 storeys or a 12m height measured from the average lot grade.

### **PARKING**

Parking in the R1WS District will be via front attached garage and driveway. As per the City's LUB, two on-lot parking spaces will be provided per home. Additional parking will be available on-street.

### **ACCESS**

Access to R1WS homes will be via the front street. A portion of these lots will also be accessible via the rear lane for municipal servicing purposes; this will also allow for additional rear parking or storage of recreation vehicles.

### **SECONDARY SUITES**

*Multiplex buildings commonly range from 4 to 18 units and share no common spaces: all units have separate entries and utilities.*

### **Row homes**

*Row homes consist of three or more attached units which do not overlap one another and have shared common walls from foundation to roofs.*

### **Additional Suites**

*As per the City's zoning, additional suites will be accommodated as a discretionary use in the R-H district.*

### **DENSITY**

*The anticipated density in this district is approximately 85.0 du/ha for apartments of multi-unit buildings, assuming a four-storey building with underground parking. The density for rowhomes is approximately 35.0 du/ha.*

### **HEIGHT**

*Should the R3 site be developed as one or more multi-family building, the maximum height is 6 storeys; however, if the R3 site is developed for row homes, the maximum height is 3 storeys or a 15m from the average lot grade.*

### **PARKING & ACCESS**

*All parking will be determined by what type of units are constructed; however, all will be located on-site as per the City of Red Deer's Zoning Bylaw. The primary access to the R-H sites will be via an internal roadway network; however, front street access will be provided for pedestrian and visitor use.*

Although secondary suites are permitted in this land use district; most R1WS housing in Evergreen will not have rear lanes which will not allow for the additional parking required by secondary suites.

### **R1G Small Lot Front Garage**

Small Lot housing with attached front garages offer a more affordable attached-garage housing option to residents due to the decreased lot width. At a minimum of 10.5m, housing in this district has less interaction between the house and streetscape; for this reason architectural controls are important to enhance the visual appeal of the garage.

### **APPROPRIATE HOUSING TYPES**

Housing types in this district could include bi-level, or two-storey homes.

### **DENSITY**

The maximum density allowable in this district is 31.2 du/ha; however, it is estimated that density will be approximately 25.4 du/ha. This assumption is based on an assumed average lot size of 393.7m<sup>2</sup>.

### **HEIGHT**

The maximum height for homes in this district will be 2 storeys or a 10m height measured from the average lot grade.

### **PARKING**

Parking in the R1G District will be via front attached garage and driveway. As per the City's LUB, two on-lot parking spaces will be provided per home.

### **ACCESS**

*\*Graphic representations of buildings alongside above text adjusted to better align with changes to text\**

Access to R1G homes will be via the front street; however, some will also be accessible via the rear lane for municipal servicing purposes; this will also allow for additional rear parking or storage of recreation vehicles.

### **SECONDARY SUITES**

As per the City's LUB, secondary suites are not permitted in this land use area. Corner lots within a portion of the R1G areas in the southwest have been identified as R1 Residential (Low Density) to accommodate the potential of secondary suites. Lots identified include those on corner lots, with lanes, or other areas with additional parking capacity.

### **Multi-Family**

#### **R1A Semi-Detached**

Semi-detached housing is a popular housing option for those looking for a more affordable solution that still offers a private yard. These houses are constructed as two independent dwelling units attached side-by-side with a common wall extending from the foundation to the roof.

Semi-detached homes have been located throughout the Evergreen neighbourhood. This type of housing will be located in the areas that provide for an ideal housing transition from higher density to single family homes.

### **APPROPRIATE HOUSING TYPES**

Appropriate housing types for this district include bungalow, bi-level, or two-storey semi-detached housing. These units may or may not include front garages.



**DENSITY**

The maximum density allowable in this district is 43 du/ha; however, it is anticipated that the density will be approximately 29.2 du/ha. This assumption is based on an assumed average lot size of 342.04m<sup>2</sup>.

**HEIGHT**

The maximum height for homes in this district will be 2 storeys or a 10m height measured from the average lot grade.

**PARKING**

The majority of parking in the R1A district will be via front attached garage and driveway. As per the City's LUB, two parking spaces will be provided per home.

**ACCESS**

Access to R1A homes will be via the front street. Those lots located along the west boundary of the Plan Area will also be accessible via the rear lane for municipal servicing purposes; this will also allow for additional rear parking or storage of recreation vehicles.

**SECONDARY SUITES**

As per the City's LUB, secondary suites are not permitted in this land use area.

**R2T Townhouse/Row Homes**

As based on the definition provided by Statistics Canada, row homes are essentially three or more dwellings joined side-by-side which do not have dwellings either above or below. Often called town houses, these structures include common walls extending from the foundation to the roof.



Row homes provide an alternative and more affordable housing form suitable to a range of residents. Row homes will be located along the gateway collector roadway to provide massing and a continuous, consistent building form with high architectural controls.

#### **APPROPRIATE HOUSING TYPES**

Appropriate housing types for this district include bungalow, bi-level, two-storey, or two and half storey row homes. These units may be developed in a tradition row fashion or clustered condo-style development with an internal roadway. R2T homes may or may not include front garages.

#### **DENSITY**

The maximum density allowable in this district is 54.0 du/ha; however, it is anticipated that the density will be approximately 35.0 du/ha. This assumption is based on the City of Red Deer's recommendations.

#### **HEIGHT**

The minimum height for homes in this district is 2 storeys, with a maximum height of 2.5 storeys or a 12m height measured from the average lot grade.

#### **PARKING**

Townhomes along Evergreen Way will be at the rear of the lot. As per the City's LUB, two parking spaces will be provided per home. Rear lanes in these areas will be paved to facilitate and encourage residents to utilize rear parking.

Limited protected parking will be provided along the main gateway roadway to provide some visitor parking to guests of homeowners along this road as continuous on-street parking will not be permitted.

#### **ACCESS**

Access to R2T homes will be primarily via the rear lane where garages will be located. The majority of these lots will also be accessible via the front street which will allow for pedestrian access and guest parking.

#### **SECONDARY SUITES**

As per the City's LUB, secondary suites are not permitted in this land use area.

#### **R2 Medium Density**

The medium density residential district is intended to provide a range of compatible housing types such as single detached homes and multi-unit buildings.

#### **APPROPRIATE HOUSING TYPES**

Housing types in this area may include single family dwellings, semi-detached structures, multi-attached, or multi-family buildings.

#### **DENSITY**

The maximum density allowable in this district is 54.0 du/ha; however, it is anticipated that the density will be approximately 36.4 du/ha. This density is an average of densities associated with the possible housing types envisioned for development in this area (semi-detached, row homes, or low rise multi-family buildings). The actual density will be completely dependant on which type of housing is constructed.

### HEIGHT

The maximum height for single detached homes in this area is 2 storeys or a 10m height measured from the average lot grade; however, multi-family buildings may by over this limit.

### PARKING

All parking will be determined by what type of units are constructed; however, all will be located on-site as per the City of Red Deer’s Land Use Bylaw.

### ACCESS

Access to the R2 sites will be via the front roadway.

### SECONDARY SUITES

Secondary suites will be permitted in detached dwelling units only. Secondary suites require 1 parking space for suites with 2 or fewer bedrooms, and 2 parking spaces for suites with greater than 2 bedrooms.

### R3 Multiple Family

R3 Multiple Family areas can take a variety of forms as further described below. This type of housing provides a more affordable residential option that appeals to a variety of residents including but not limited to renters, first time home buyers, and retirees. One R3 area has been located in Evergreen at the terminus of the gateway roadway as part of the community node. This location was chosen based on the anticipated location of public transit stops and proximity to the open space network and community amenities.

### APPROPRIATE HOUSING TYPES

This R3 multi-unit housing will be constructed based on market conditions at the time of development and could include one of the following. All building types listed below can take the form of rental or condo facilities. Regardless of building type, this site should be designed as a focal feature of the community with a high level of architectural detail and visual interaction at street-level.

***Apartment-Style***

Apartment style buildings have shared entries, hallways, and often building amenities such as fitness centres or hot tubs.

***Multiplex Building***

Multiplex buildings commonly range from 4 to 18 units and share no common spaces: all units have separate entries and utilities.

***Row homes***

Row homes consist of three or more attached units which do not overlap one another and have shared common walls from foundation to roofs.

**DENSITY**

The anticipated density in this district is approximately 85.0 du/ha for apartments of multi-unit buildings, assuming a four-storey building with underground parking. The density for rowhomes is approximately 35.0 du/ha.

**HEIGHT**

Should the R3 site be developed as one or more multi-family building, the maximum height is 4 storeys; however, if the R3 site is developed for row homes, the maximum height is 2 storeys or a 10m from the average lot grade.

### **PARKING & ACCESS**

All parking will be determined by what type of units are constructed; however, all will be located on-site as per the City of Red Deer's Land Use Bylaw. The primary access to the R3 sites will be via an internal roadway network; however, front street access will be provided for pedestrian and visitor use.

### **SECONDARY SUITES**

As per the City's LUB, secondary suites will not be accommodated in the R3 District.

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### **Density and Units**

The estimated population of Evergreen is 2,161 persons. This number is based on the land uses proposed and will change after final build-out. The overall housing density of the Evergreen community is 17.5 du/net developable hectare. Full land use calculations are located on page 31.

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### **Density and Units**

The estimated population of Evergreen is ~~2,161~~2,636 persons. This number is based on the land uses proposed and will change after final build-out. The overall housing density of the Evergreen community is ~~17.5~~20.7 du/net developable hectare. Full land use calculations are located on page ~~31~~ 24.

*\*Table 1 from current NASP page 18 combined with Table 2 from current NASP Page 28 to create new Table 1 with adjusted calculations\**

Table 2 - Residential Land Use Calculations

Land Use	Area (ha)	Units	Density	% Of Housing Stock
R1 Residential (Low Density) District	12.80	294	23.0	32.6%
*Secondary Suites	--	44	0.0	4.9%
R1G Residential (Small Lot) District	7.00	177	25.4	19.6%
R1WS Residential (Wide/Shallow Lot) District	4.06	112	27.6	12.4%
R1A Residential (Semi-Detached Dwelling)	2.27	65	29.2	7.2%
R2 Residential (Medium Density) District	2.79	101	36.4	11.2%
R2T Residential (Town House) District	2.31	80	35.0	8.9%
R3 Residential (Multiple Family) District	0.34	29	85.0	3.2%
<b>Total</b>	<b>31.58</b>	<b>902</b>		<b>100.0%</b>

\*Calculated assuming that 15% of all R1 units will include secondary suites; these suites are counted as 1 unit each.

Densities calculations are based on the assumptions as outlined following assumed averages, exact density and unit counts may differ at the time of subdivision. :

- R1 Average lot size: 435.2m<sup>2</sup>
- R1G Average lot size: 393.7m<sup>2</sup>
- R1WS Average lot size: 362.1m<sup>2</sup>
- R1A Average lot size: 342.04m<sup>2</sup>
- R2 36.4 du/ha - housing type may vary; density average of possible housing types anticipated (R1A, R2T, R3-3storey)
- R2T 35.0 du/ha - as per the City of Red Deer's Planning Guidelines
- R3 85.0 du/ha - estimated density for a four storey building with underground parking

Table 3 - Population Projection

Residential Land Use Calculation	Hectares	Acres	# of Units	Household Size	Population
Single Detached Residential	23.86	58.97	627		1502
R1 Residential Low Density	12.80	31.64	294	2.4	705
*R1 Secondary Suites (15% of R1 units)	0.00	0.00	44	2.4	105
R1G Residential Small Lot	7.00	17.29	177	2.4	424
R1WS Wide Shallow Lot	4.06	10.04	112	2.4	268
Semi-Detached and Multi-Family Residential	7.71	19.06	275		659
R1A Residential Semi-Detached	2.27	5.61	65	2.4	156
R2 Residential Medium Density	2.79	6.89	101	2.4	242
R2T Residential Town House	2.31	5.70	80	2.4	192
R3 Residential Multiple Family	0.34	0.85	29	2.4	69
<b>Total</b>	<b>31.58</b>	<b>78.03</b>	<b>902</b>		<b>2,161</b>
<b>Density: 17.5 du/ha</b> (902 Dwelling Units / 51.63 Developable Hectares)					

Table 1 - Residential Land Use Calculations

Land Use	Area (ha)	Units	Density	% Of Housing Stock
R-L Residential Low-Density Zone	5.17	118	23.0	10.7%
* R-L Additional Suites	--	17	0.0	1.5%
R-N Residential Narrow Lot Zone	11.13	305	27.8	27.7%
* R-N Additional Suites	--	45		4.1%
R-W Residential Wide Zone	4.06	117	29.0	10.6%
* R-W Additional Suites	--	17		1.5%
R-D Residential Duplex Zone	3.98	149	37.7	13.5%
* R-D Additional Suites	--	22		2.0%
R-M Residential Medium-Density Zone	2.77	97	35.0	8.8%
R-M Residential Medium-Density Zone (Townhome)	4.14	180	43.5	16.3%
R-H Residential High-Density Zone	0.69	34	50.0	3.1%
<b>Total</b>	<b>31.95</b>	<b>1,101</b>		<b>100.0%</b>

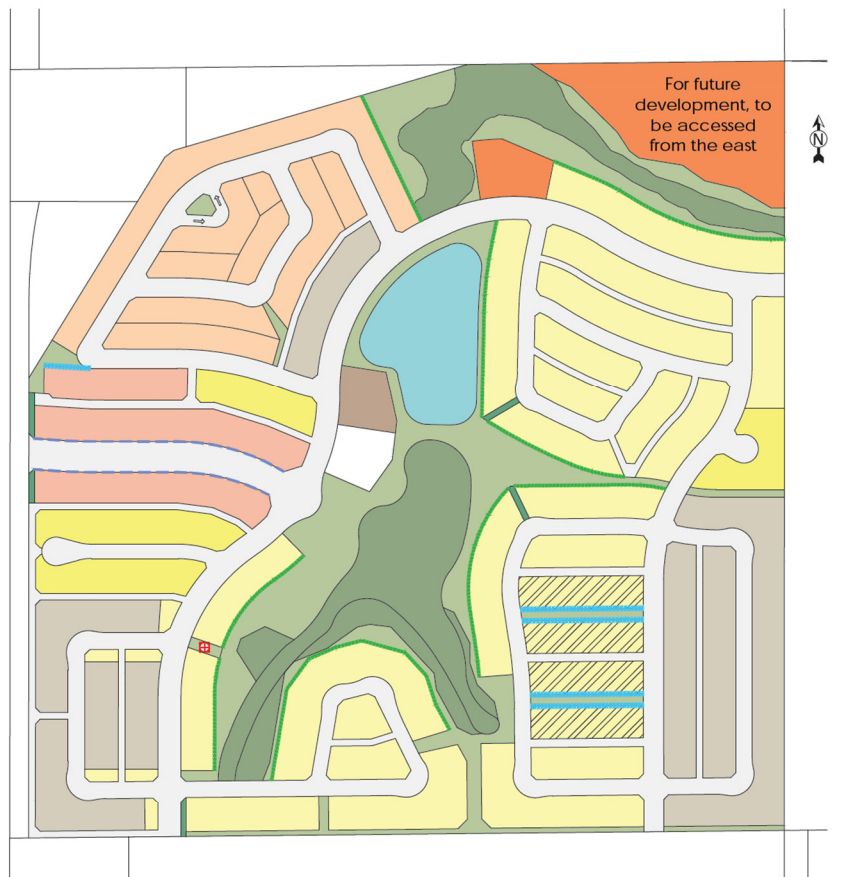
\*Calculated assuming that 15% of all R-L, R-N, R-W & R-D units will include additional suites; these suites are counted as 1 unit each.

Densities calculations are based on the assumptions as outlined following assumed averages, exact density and unit counts may differ at the time of subdivision. :

Table 2 - Population Projection

Residential Land Use Calculation	Hectares	# of Units	Household Size	Population
R-L Residential Low-Density Zone	5.17	118	2.4	283
* R-L Additional Suites	-	17	2.4	40
R-N Residential Narrow Lot Zone	11.13	305	2.4	732
* R-N Additional Suites	-	45	2.4	108
R-W Residential Wide Zone	4.06	117	2.4	280
* R-W Additional Suites	-	17	2.4	40
R-D Residential Duplex Zone	3.98	149	2.4	356
* R-D Additional Suites	-	22	2.4	52
R-M Residential Medium-Density Zone	2.77	97	2.4	232
R-M Residential Medium-Density Zone (Townhome)	4.14	180	2.4	432
R-H Residential High-Density Zone	0.69	34	2.4	81
<b>Total</b>	<b>31.95</b>	<b>1,101</b>		<b>2,636</b>
<b>Density: 20.5 du/ha</b> (1,101 Dwelling Units / 53.77 Developable Hectares)				

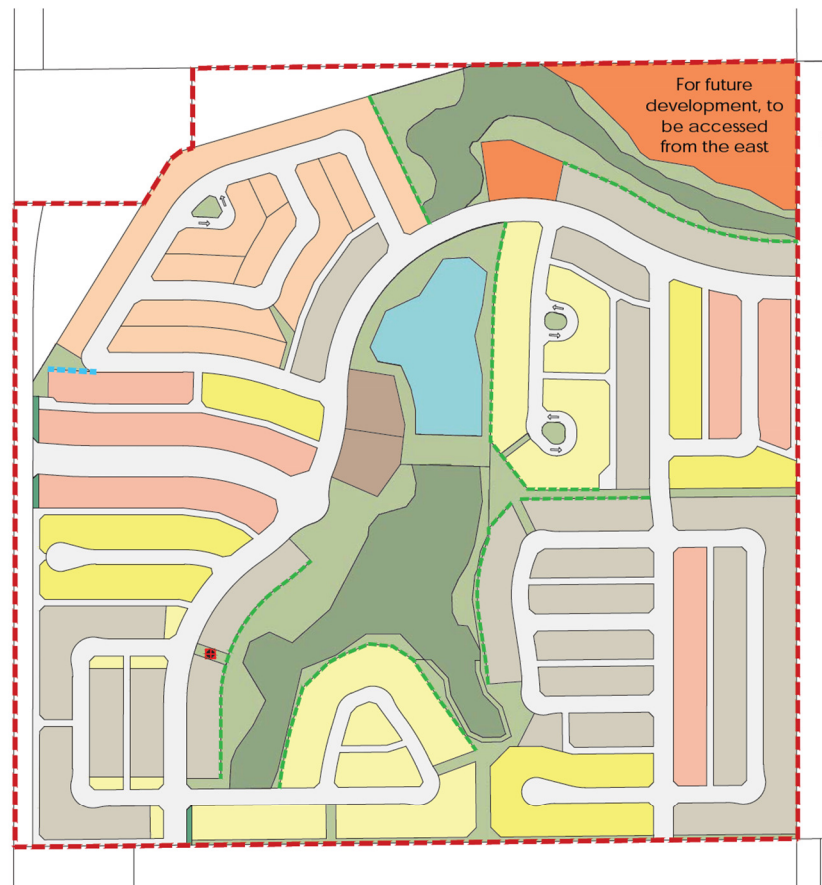
Figure 9 - Residential Concept Plan



**Legend**

- |                    |                            |                             |
|--------------------|----------------------------|-----------------------------|
| R1 Low Density     | R2 Medium Density          | Public Utility Lot (PUL)    |
| R1 Reverse Housing | R3 Multiple Family         | Stormwater Management (PUL) |
| R1WS Wide Shallow  | Fronting onto Green Space  |                             |
| R1G Small Lot      | Possible Walk-Out Basement |                             |
| R1A Semi-Detached  | Municipal Reserve (MR)     |                             |
| R2T Town House     | Environmental Reserve (ER) |                             |

Figure 9 - Residential Concept Plan

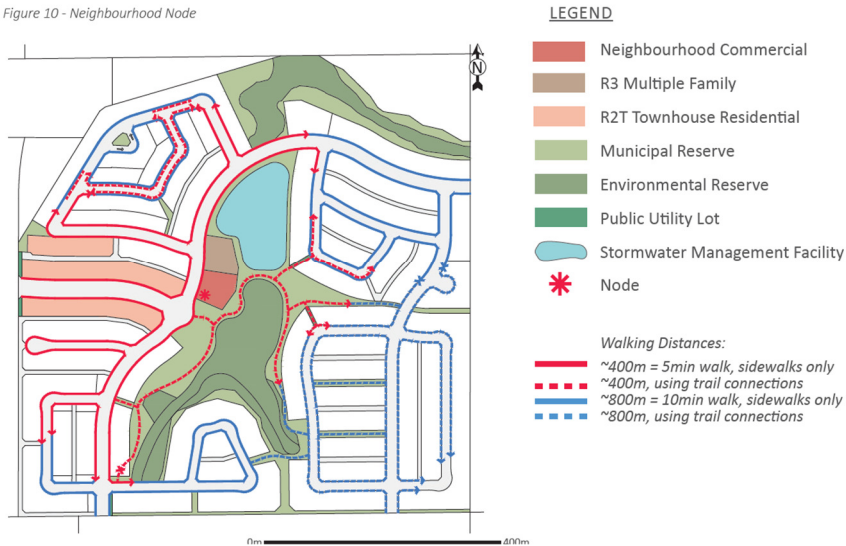


**Legend**

- |   |                              |
|---|------------------------------|
| R-L Residential Low Density                 | R-H Residential High-Density |
| R-W Residential Wide                        | Fronting onto Green Space    |
| R-N Residential Narrow Lot                  | Possible Walk-Out Basement   |
| R-D Residential Duplex                      | Stormwater Management (PUL)  |
| R-M Residential Medium-Density              |                              |
| R-M Residential Medium-Density (Town House) |                              |



Figure 10 - Neighbourhood Node



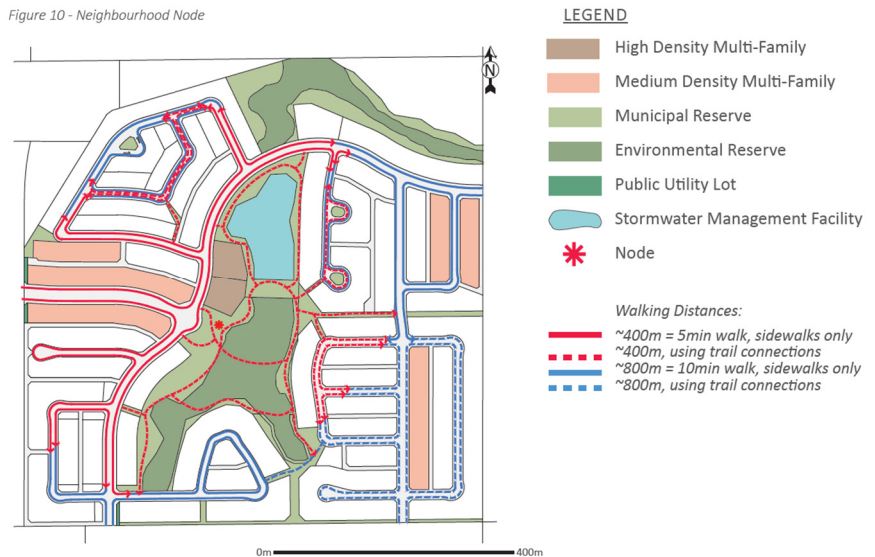
## Neighbourhood Node

Evergreen's neighbourhood node consists of a variety of uses with street-orientation to provide an array of services to residents and an opportunity for neighbours to socialize. The central park site forms the basis of this node which is supported by commercial, higher density residential, and community type uses. The location of this node was chosen to facilitate both pedestrian and vehicular access.

## Commercial Use

A neighbourhood commercial area has been identified at the terminus of the primary gateway road. This location was chosen for it's proximity to vehicular and pedestrian traffic routes, and to form part of a community node. The types of commercial businesses envisioned for this area include: a coffee shop, small restaurant, doctors office, child minding, hair salon, or similar.

Figure 10 - Neighbourhood Node



## Neighbourhood Node

Evergreen's neighbourhood node consists of a variety of uses with street-orientation to provide an array of services to residents and an opportunity for neighbours to socialize. The central park site forms the basis of this node which is supported by **commercial**, higher density residential, and community type uses. The location of this node was chosen to facilitate both pedestrian and vehicular access.

## Commercial Use

**A neighbourhood commercial area has been identified at the terminus of the primary gateway road. This location was chosen for it's proximity to vehicular and pedestrian traffic routes, and to form part of a community node. The types of commercial businesses envisioned for this area include: a coffee shop, small restaurant, doctors office, child minding, hair salon, or similar.**



Site and building design for this commercial area will be guided using the City's LUB and architectural controls created by the Developer. The location of parking and use of screening should be specifically addressed to minimize visual impact from the roadway and rear walking trails.

Should this area not be successfully sold for its intended purpose, this site will be converted to an R3 use.

### Community Amenity Sites

Two community amenity sites have been located within the Evergreen neighbourhood to accommodate the possible development of community uses such as: temporary care, assisted living, adult or regular day care, place of worship, or other uses proposed and approved by the City.

Both sites have been located as part of the Evergreen community node in close proximity to walking trails and transit stops to facilitate their use. These sites will be advertised for sale through local media and the City of Red Deer website for one year; if the sites are not purchased for their intended use, they will be developed as R3 multi-family or R1WS residential development.

If both the central community amenity site and neighbourhood commercial site are not sold for their intended purpose, and both are rezoned to R3, the two sites may be combined into one R3 site.

*\* The community Amenity Sites were advertised for sale following the approval of the Evergreen NASP in accordance with the City of Red Deer's guidelines. Ultimately, neither sites were purchased or*

~~Site and building design for this commercial area will be guided using the City's LUB and architectural controls created by the Developer. The location of parking and use of screening should be specifically addressed to minimize visual impact from the roadway and rear walking trails.~~

~~Should this area not be successfully sold for its intended purpose, this site will be converted to an R3 use.~~

### ~~Community Amenity Sites~~

~~Two community amenity sites have been located within the Evergreen neighbourhood to accommodate the possible development of community uses such as: temporary care, assisted living, adult or regular day care, place of worship, or other uses proposed and approved by the City.~~

~~Both sites have been located as part of the Evergreen community node in close proximity to walking trails and transit stops to facilitate their use. These sites will be advertised for sale through local media and the City of Red Deer website for one year; if the sites are not purchased for their intended use, they will be developed as R3 multi-family or R1WS residential development.~~

~~If both the central community amenity site and neighbourhood commercial site are not sold for their intended purpose, and both are rezoned to R3, the two sites may be combined into one R3 site.~~

~~\*The community Amenity Sites were advertised for sale following the approval of the Evergreen NASP in accordance with the City of Red Deer's guidelines. Ultimately, neither sites were purchased or~~

developed for community amenity uses. In May 2017 both sites were rezoned to their alternate land uses.

## Page 31

Table 4 - Land Use Calculations

Land Use Category	Hectares	Acres	% of Developable Area	# of Dwelling Units
Gross Plan Area	62.25	153.83		
Environmental Reserve	5.06	12.50		
Major Streets (Expressway)	2.59	6.41		
Major Streets (30th Ave)	0.94	2.32		
Commercial	0.35	0.86		
Stormwater Management Facility SWMF (PUL)	1.69	4.17		
Developable Plan Area	51.63	127.57	100.00%	
<b>Residential</b>	<b>31.58</b>	<b>78.03</b>	<b>61.2%</b>	<b>902</b>
R1 Residential (Low Density) District	12.80	31.64	24.8%	294
R1 Secondary Suites (15% of R1 units)	0.00	0.00	0.0%	44
R1G Residential (Small Lot) District	7.00	17.29	13.6%	177
R1WS Residential (Wide/Shallow Lot) District	4.06	10.04	7.9%	112
R1A Residential (Semi-Detached Dwelling)	2.27	5.61	4.4%	65
R2 Residential (Medium Density) District	2.79	6.89	5.4%	101
R2T Residential (Town House) District	2.31	5.70	4.5%	80
R3 Residential (Multiple Family) District	0.34	0.85	0.7%	29
<b>Other</b>	<b>20.05</b>	<b>17.89</b>	<b>38.8%</b>	
<b>Open Space</b>	<b>6.55</b>	<b>16.18</b>	<b>12.7%</b>	
Municipal Reserve (MR)	6.44	15.90	11.3%	
Public Utility Lot Excluding SWMF (PUL)	0.11	0.28	0.2%	
<b>Transportation</b>	<b>13.50</b>	<b>33.36</b>	<b>26.2%</b>	
Collector Roadways	3.61	8.93	7.0%	
Local Roadways	7.85	19.40	15.2%	
Lanes	2.04	5.03	3.9%	
<b>Other Uses</b>	<b>0.00</b>	<b>0.00</b>	<b>0.0%</b>	
Emergency Services Site	0.00	0.00	0.0%	
Institutional Service Facility	0.00	0.00	0.0%	

\* MR calculation is a % of Gross Plan Area less Environmental Reserve.

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developed for community amenity uses. In May 2017 both sites were rezoned to their alternate land uses:

*\*Supporting example graphics are also removed from the section\**

## Page 24

Table 3 - Land Use Calculations

Land Use Category	Hectares	Acres	% of Developable Area
Gross Plan Area	62.26	153.85	
Environmental Reserve	4.96	12.25	
Major Streets (Expressway)	2.20	5.44	
Major Streets (30th Ave)	1.33	3.29	
Developable Plan Area	53.77	132.87	100.00%
<b>Residential</b>	<b>31.95</b>	<b>78.94</b>	<b>61.0%</b>
R-L Residential Low-Density Zone	5.17	12.77	9.9%
R-L Additional Suites (15% of R-L units)	--	--	--
R-N Residential Narrow Lot Zone	11.13	27.50	21.3%
R-N Additional Suites (15% of R-N units)	--	--	--
R-W Residential Wide Zone	4.06	10.04	7.8%
R-L Additional Suites (15% of R-L units)	--	--	--
R-D Residential Duplex Zone	3.98	9.84	7.6%
R-D Additional Suites (15% of R-D units)	--	--	--
R-M Residential Medium-Density Zone	6.91	17.08	13.2%
R-M Residential High-Density Zone	0.69	1.71	1.3%
<b>Open Space</b>	<b>7.88</b>	<b>19.48</b>	<b>12.4%</b>
Municipal Reserve (MR)	6.38	15.76	11.1%
Public Utility Lot Excluding SWMF (PUL)	0.07	0.16	0.1%
Public Utility Lot SWMF (PUL)	1.44	3.56	2.7%
<b>Transportation</b>	<b>13.94</b>	<b>34.45</b>	<b>25.9%</b>
Collector Roadways	3.69	9.12	6.9%
Local Roadways	8.10	20.01	15.1%
Lanes	2.15	5.32	4.0%

Densities calculations are based on the assumptions as outlined following assumed averages, exact density and unit counts may differ at the time of subdivision. :

- R-L Average lot size: 435m<sup>2</sup>
- R-N Average lot size: 360m<sup>2</sup>
- R-W Average lot size: 345m<sup>2</sup>
- R-D Average lot size: 265m<sup>2</sup>

## Page 25

Table 5 - Roads and Utilities Percentage

Roads and Utilities	Hectares	Acres	% of Net Plan Area
Gross Plan Area	62.25	153.83	
<i>Environmental Reserve</i>	5.06	12.50	
Net Plan Area	57.20	141.33	100.0%
Allowable Area for Roads & Utilities	17.16	42.40	30.0%
Actual Area of Roads & Utilities	18.84	46.54	32.9%
Municipal Improvements	3.53	8.73	6.2%
<i>Northland Drive Expressway</i>	2.59	6.41	4.5%
<i>30th Avenue Arterial</i>	0.94	2.32	1.6%
Evergreen Roadways and Utilities	15.21	37.59	26.6%
<i>Collector Roadways</i>	3.61	8.93	6.3%
<i>Local Roadways</i>	7.85	19.40	13.7%
<i>Lanes</i>	2.04	5.03	3.6%
<i>Public Utility Lots</i>	1.80	4.45	3.1%
Over Dedication	1.68	4.14	2.9%

Table 4 - Roads and Utilities Percentage

Roads and Utilities	Hectares	Acres	% of Net Plan Area
Gross Plan Area	62.26	153.85	
<i>Environmental Reserve</i>	4.96	12.25	
Net Plan Area	57.31	141.60	100.0%
Allowable Area for Roads & Utilities	17.19	42.48	30.0%
Actual Area of Roads & Utilities	18.98	46.90	33.1%
Municipal Improvements	3.53	8.73	6.2%
<i>Northland Drive Expressway</i>	2.59	6.41	4.5%
<i>30th Avenue Arterial</i>	0.94	2.32	1.6%
Evergreen Roadways and Utilities	15.45	38.17	27.0%
<i>Collector Roadways</i>	3.69	9.12	9.0%
<i>Local Roadways</i>	8.10	20.01	14.1%
<i>Lanes</i>	2.15	5.32	3.8%
<i>Public Utility Lots</i>	1.51	3.72	2.6%
Over Dedication	1.79	4.41	2.6%

## Overview

Evergreen has been developed with 32.85 ac (13.30 ha) of public open spaces, 21% of the total Plan Area. Four Different types of open spaces make up the overall open space network as indicated in **Table 6 – Green Space Amenities**.

Table 6 - Green Space Amenities

Green Space		Total No. of Parcels	Area (ha)	Area (ac)
Open Spaces	Neighbourhood Park (Including SWMWF)	2	4.66	11.51
	Parquettes	1	0.05	0.12
Natural Areas		8	5.95	14.70
Linear Parks & Pedestrian Links		17	2.64	6.53
TOTAL		28	13.30	32.85

Table 7 - Municipal Reserve Calculation

Municipal Reserve Dedication	Acres	Hectares	% of MR Developable Area
Gross Plan Area	62.25	153.83	
Environmental Reserve	5.06	12.50	
MR Developable Area	57.20	141.33	100.0%
Required MR Dedication	5.72	14.13	10.0%
Actual MR Dedication	6.44	15.90	11.3%

It is recognized that Evergreen has dedicated 11.3% of its MR Developable Area as Municipal Reserve; however the City of Red

## Overview

Evergreen has been developed with ~~32.85 ac (13.30 ha)~~ **31.73 ac (12.84 ha)** of public open spaces, 21% of the total Plan Area. Four Different types of open spaces make up the overall open space network as indicated in **Table 6 5 – Green Space Amenities**.

Table 5 - Green Space Amenities

Green Space		Area (ha)	Area (ac)
Open Spaces	Neighbourhood Park (Including SWMWF)	4.09	10.10
	Parquettes	0.05	0.12
Natural Areas		5.98	14.77
Linear Parks & Pedestrian Links		2.72	6.71
TOTAL		12.84	31.73

Table 6 - Municipal Reserve Calculation

Municipal Reserve Dedication	Acres	Hectares	% of MR Developable Area
Gross Plan Area	62.26	153.85	
Environmental Reserve	4.96	12.25	
MR Developable Area	57.31	141.60	100.0%
Required MR Dedication	5.73	14.16	10.0%
Actual MR Dedication	6.38	15.76	11.1%

It is recognized that Evergreen has dedicated ~~11.3%~~ **11.1%** of its MR Developable Area as Municipal Reserve; however the City of Red Deer



Figure 11 - Types of Open Spaces and Amenities





Figure 12 - Open Space and Trail Network

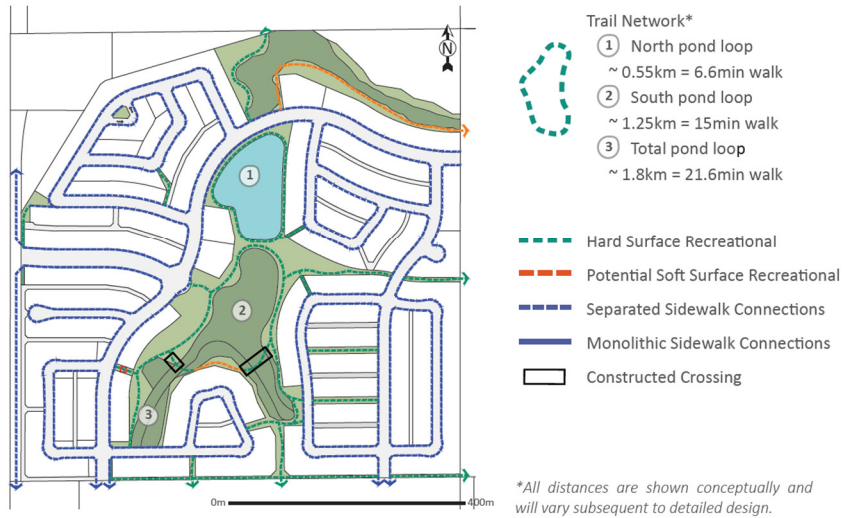
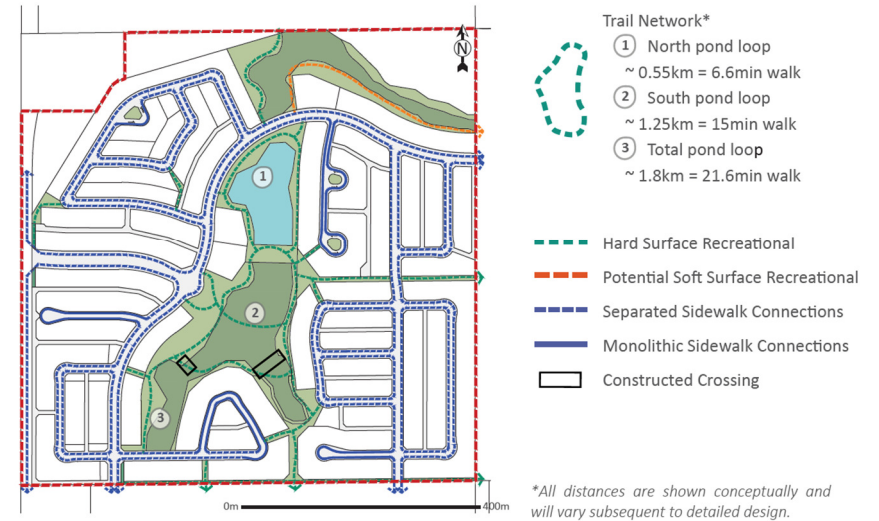
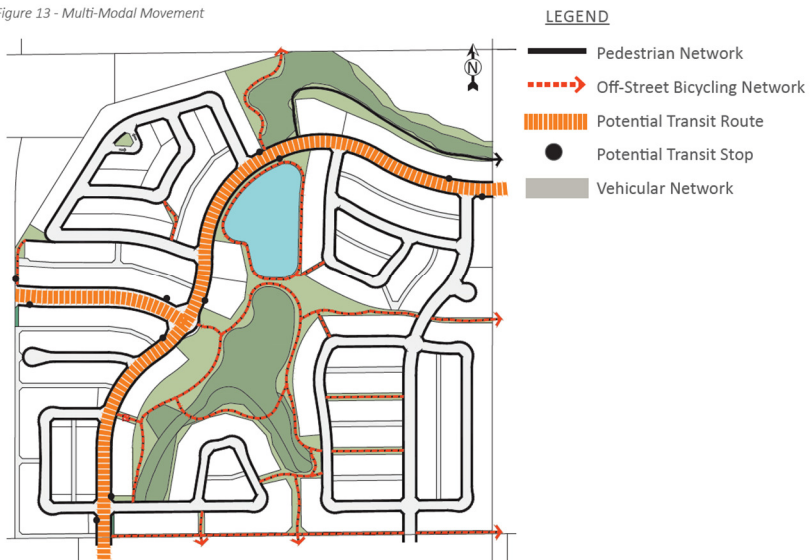


Figure 12 - Open Space and Trail Network



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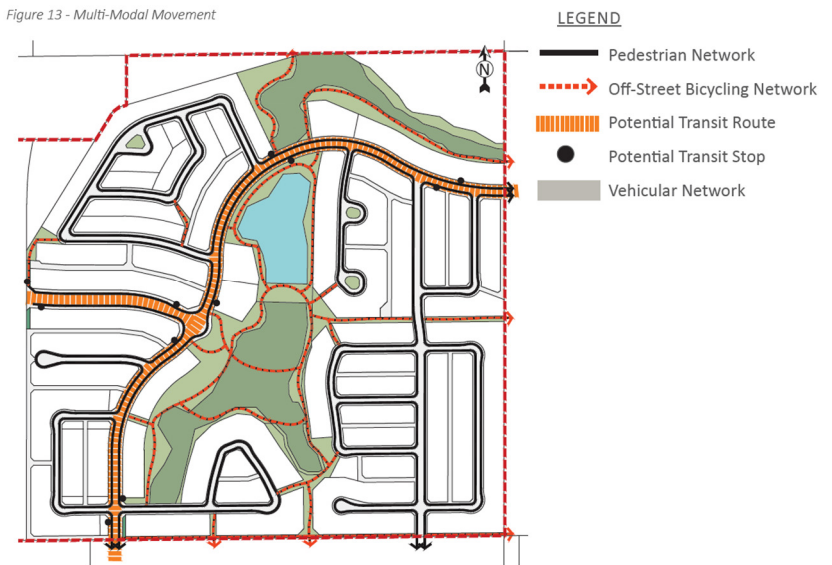
Figure 13 - Multi-Modal Movement



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Figure 13 - Multi-Modal Movement



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Figure 14 - Sidewalk Type

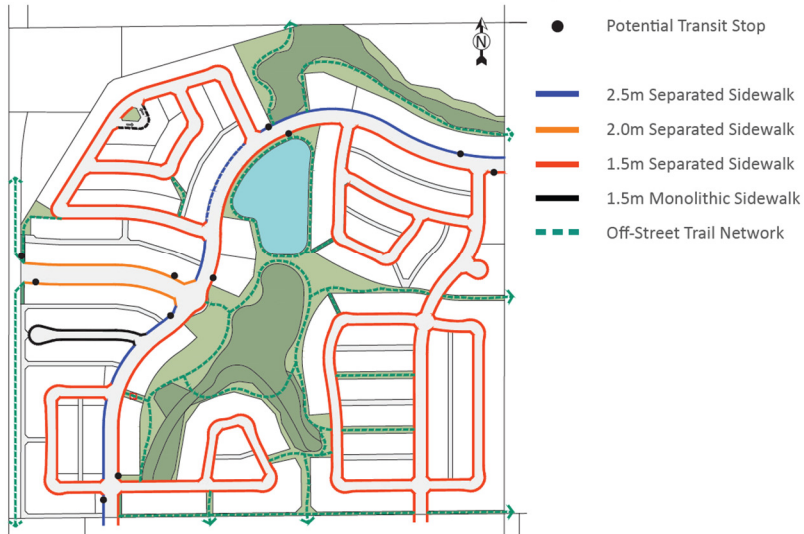


Figure 14 - Sidewalk Type

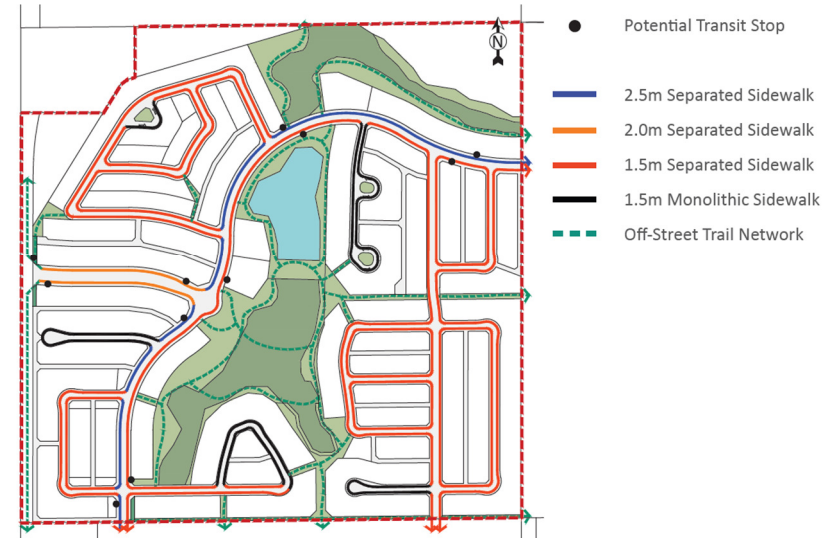


Figure 15 - Transit Network

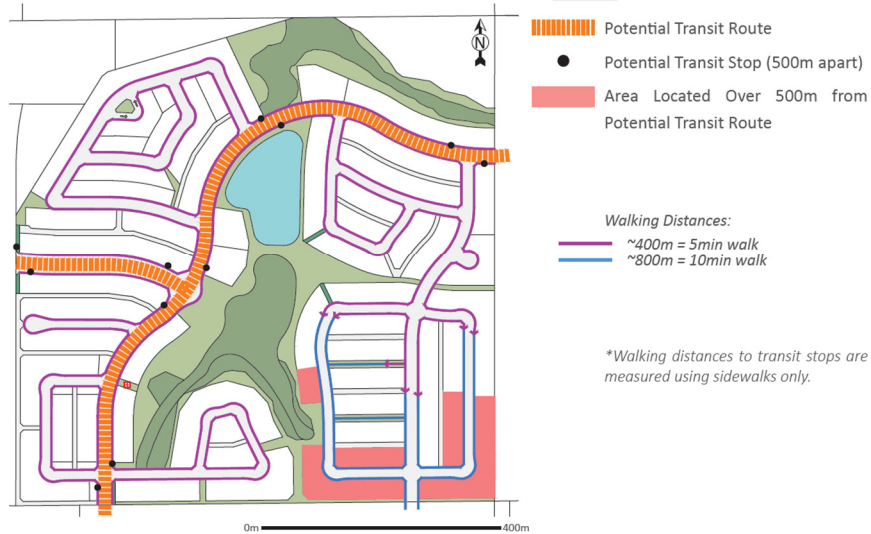


Figure 15 - Transit Network

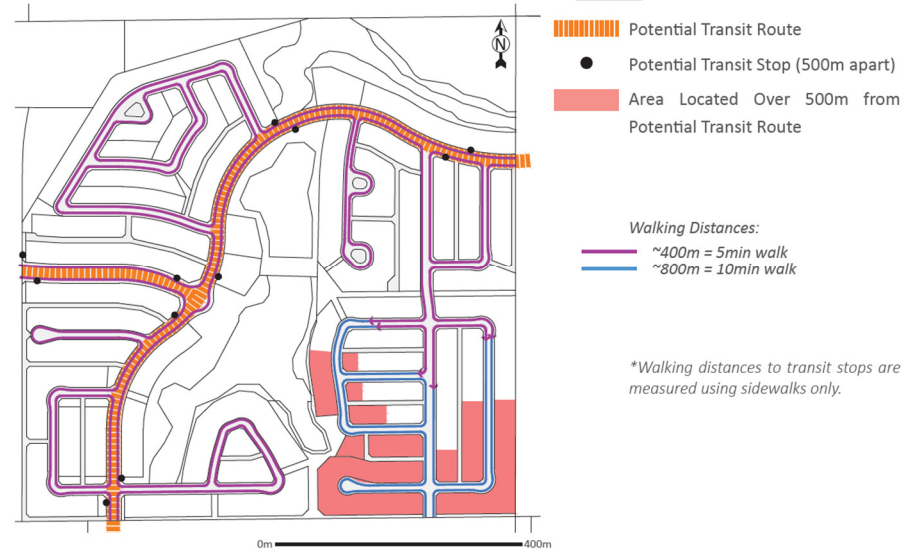


Figure 16 - Pedestrian Safety



### Important Trail Connections

Two important trail connections have been identified in the Evergreen neighbourhood. Additional trail crossings may be considered at the detailed design stage. Consideration for additional trail crossings will be based on operational requirements, pedestrian safety, and user needs.

Figure 16 - Pedestrian Safety

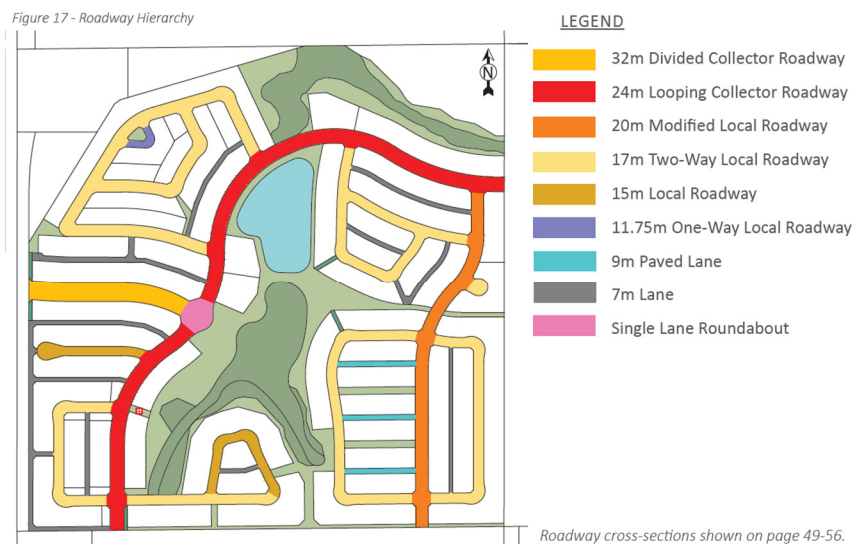


### Important Trail Connections

~~Two~~ *One* important ~~trail connections have been~~ *mid block crossing is* identified in the ~~Evergreen neighbourhood east area~~. Additional trail crossings may be considered at the detailed design stage. Consideration for additional trail crossings will be based on operational requirements, pedestrian safety, and user needs.



Figure 17 - Roadway Hierarchy



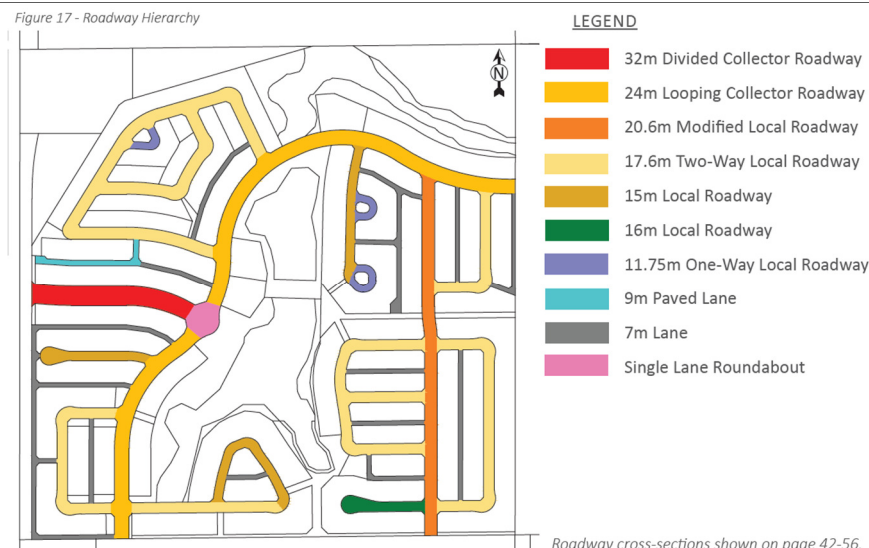
Pages 47 – 48

### 20m Modified Local Roadway

An expanded local roadway is located along the east portion of the Plan Area stretching from north to south. This roadway provides access from the main collector to residential areas east of the water body. To facilitate traffic in this area and allow the inclusion of a transit route, a 20.0m wide right-of-way with one travel lane and parking in either direction is used. This roadway also has 1.5m wide sidewalks separated from traffic by a landscaped boulevard.

As shown on **Figure 22 - 20.0m Undivided Collector Roadway Cross-Section**, this cross-section is not standard in the City of Red Deer and will be subject to review by the City prior to implementation at the Servicing Study stage.

Figure 17 - Roadway Hierarchy



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### 20.6m Modified Local Roadway

An expanded local roadway is located along the east portion of the Plan Area stretching from north to south. This roadway provides access from the main collector to residential areas east of the water body. To facilitate traffic in this area and allow the inclusion of a transit route, a ~~20.0m~~ 20.6m wide right-of-way with one travel lane and parking in either direction is used. This roadway also has 1.5m wide sidewalks separated from traffic by a landscaped boulevard.

As shown on **Figure 22 - ~~20.0m~~ 20.6m Undivided Collector Roadway Cross-Section**, this cross-section is not standard in the City of Red Deer and will be subject to review by the City prior to implementation at the Servicing Study stage.

### 17m Two-Way Local Roadway

All typical local roadways will have a 17.0m wide right-of-way with 1.5m separated sidewalks. Utilizing this cross-section in place of the City's current standard allows for an enhanced streetscape by incorporating street trees in the landscaped boulevard, separated sidewalks in either direction increasing the pedestrian experience, and increases safety by providing a buffer between pedestrians and motorists.

As shown on **Figure 23 - 15.0m/17.0m Local Roadway Cross-Section**, this cross-section is not standard in the City of Red Deer and will be subject to review by the City prior to implementation at the Servicing Study stage.

### 11.75m One-Way Local Roadway

One one-way local roadways has been used in Evergreen. Homes in the northwest portion of the Plan Area surrounding a park will utilize a road with 11.75m wide right-of-way. This roadway is shown on **Figure 24 - 11.75m One-Way Local Roadway Cross-Section** and will not have a sidewalk along the park side as a multi-use trail will be designed within the park.

### REVERSE HOUSING LANE ILLUMINATION

Special attention to the lanes utilized for reverse housing in the southeast portion of the Plan Area will be required to ensure there is clear illumination of house numbering for use by Emergency Services, delivery vehicles, taxis, etc.

The Developer will work with the City of Red Deer's Electrical Light and Power department during the Servicing Study stage to determine the most appropriate solution for providing this lighting in the lane. If

### 17.6m Two-Way Local Roadway

All typical local roadways will have a ~~17.0m~~ 17.6m wide right-of-way with 1.5m separated sidewalks. Utilizing this cross-section in place of the City's current standard allows for an enhanced streetscape by incorporating street trees in the landscaped boulevard, separated sidewalks in either direction increasing the pedestrian experience, and increases safety by providing a buffer between pedestrians and motorists.

As shown on **Figure 23 - 15.0m/~~17.0m~~ 17.6m Local Roadway Cross-Section**, this cross-section is not standard in the City of Red Deer and will be subject to review by the City prior to implementation at the Servicing Study stage.

### 16.0m Two-Way Local Roadway

*The 16.0m wide local roadway is used in one location within Evergreen that is a cul-de-sac with duplex homes. This roadway will be designed to The City of Red Deer standards which utilizes a 16.0m wide right-of-way, 11.0m wide carriage width, and includes a 1.5m wide monolithic sidewalk on each side of the roadway.*

### 11.75m One-Way Local Roadway

One one-way local roadways has been used in **three areas of** Evergreen. Homes in the northwest portion of the Plan Area surrounding a park will utilize a road with 11.75m wide right-of-way. *Two additional areas in the northeast will have similar configurations.* This roadway is shown on **Figure 24 - 11.75m One-Way Local Roadway Cross-Section** and will not have a sidewalk along the park side as a multi-use trail will be designed within the park.

### REVERSE HOUSING LANE ILLUMINATION

municipal lighting is deemed inappropriate, an architectural control will be used to require lighting be provided on the rear garages to facilitate home address recognition.

Special attention to the lanes utilized for reverse housing in the southeast portion of the Plan Area will be required to ensure there is clear illumination of house numbering for use by Emergency Services, delivery vehicles, taxis, etc.

The Developer will work with the City of Red Deer's Electrical Light and Power department during the Servicing Study stage to determine the most appropriate solution for providing this lighting in the lane. If municipal lighting is deemed inappropriate, an architectural control will be used to require lighting be provided on the rear garages to facilitate home address recognition:

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Figure 19 - 32.0m Divided Collector Roadway Cross-Section at Boulevard

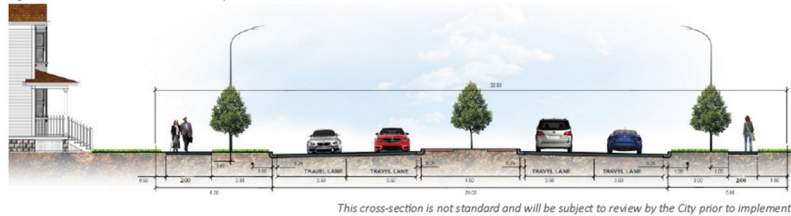


Figure 20 - 32.0m Divided Collector Roadway Cross-Section at Protected Parking

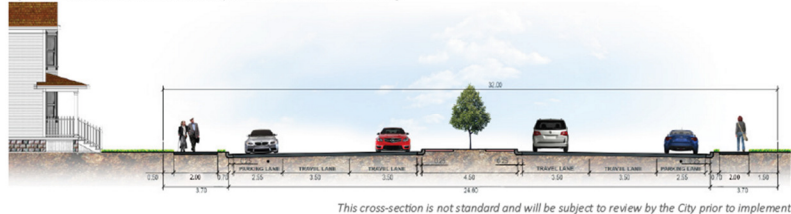
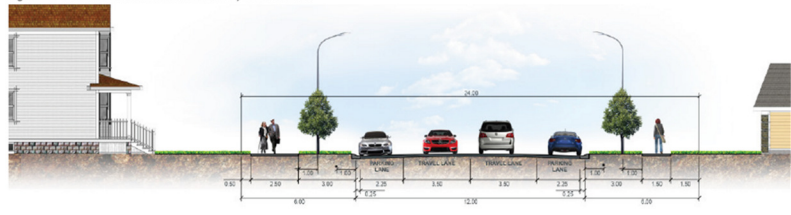


Figure 21 - 24.0m Undivided Collector Roadway Cross-Section



## Page 43

\*Some measurements have changed in figures 19, 20\*

Figure 19 - 32.0m Divided Collector Roadway Cross-Section at Boulevard

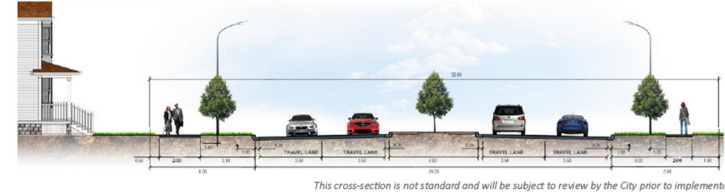


Figure 20 - 32.0m Divided Collector Roadway Cross-Section at Protected Parking

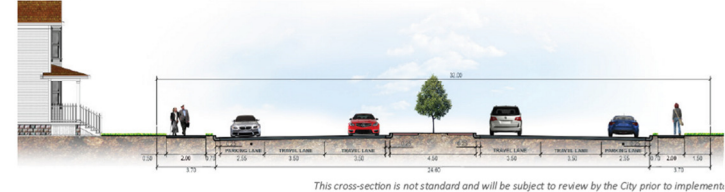
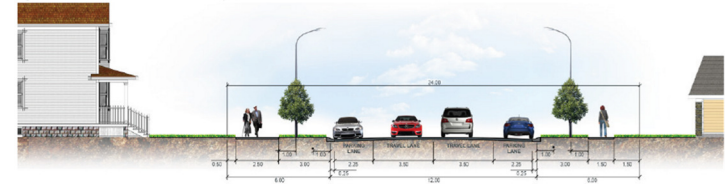
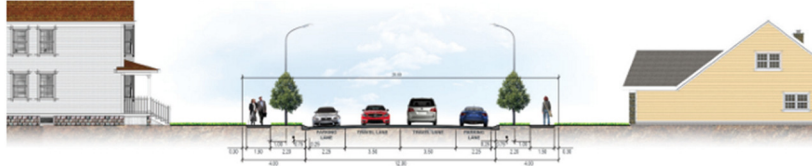


Figure 21 - 24.0m Undivided Collector Roadway Cross-Section



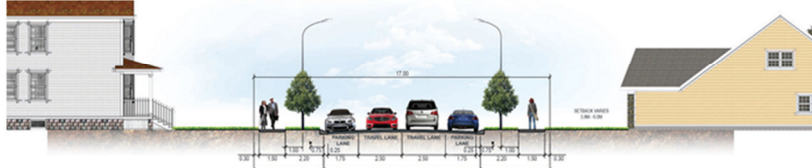
## Page 51

Figure 22 - 20.0m Undivided Collector Roadway Cross-Section



This cross-section is not standard and will be subject to review by the City prior to implementation at the Servicing Study stage.

Figure 23 - 15.0m/17.0m Local Roadway Cross-Section



This cross-section is not standard and will be subject to review by the City prior to implementation at the Servicing Study stage.

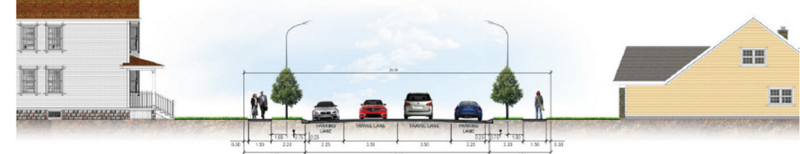


## Page 52

## Page 44

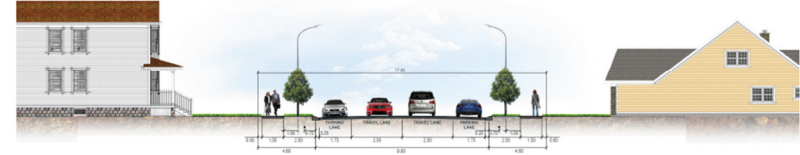
**\*Some measurements have changed in figure 23\***

Figure 22 - 20.0m Undivided Collector Roadway Cross-Section

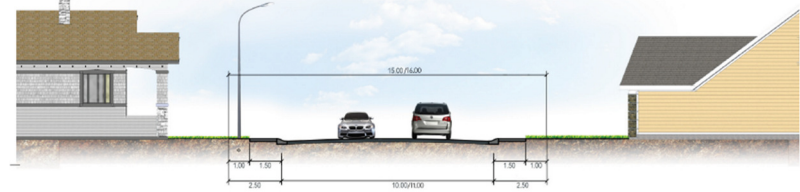


This cross-section is not standard and will be subject to review by the City prior to implementation at the Servicing Study stage.

Figure 23 - 15.0m/16.0m/17.6m Local Roadway Cross-Section



This cross-section is not standard and will be subject to review by the City prior to implementation at the Servicing Study stage.

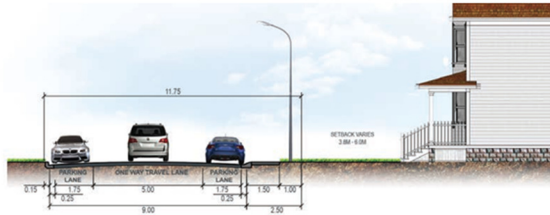


## Page 45

**\*Some measurements have changed in figure 24\***

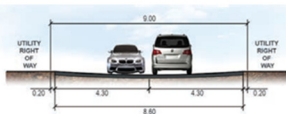


Figure 24 - 11.75m One-Way Local Roadway Cross-Section



This cross-section is not standard and will be subject to review by the City prior to implementation at the Servicing Study stage.

Figure 25 - 9.0m Paved Lane Cross-Section



This cross-section is not standard and will be subject to review by the City prior to implementation at the Servicing Study stage.

Figure 26 - 7.0m Lane Cross-Section

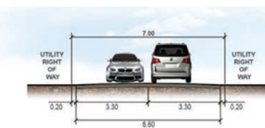
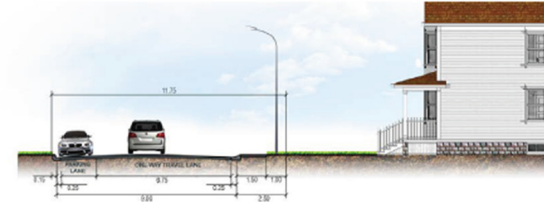
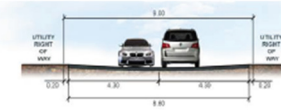


Figure 24 - 11.75m One-Way Local Roadway Cross-Section



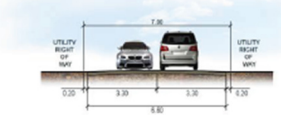
This cross-section is not standard. Parking will only be located on one side and will be subject to review by the City prior to implementation at the Servicing Study stage.

Figure 25 - 9.0m Paved Lane Cross-Section



This cross-section is not standard and will be subject to review by the City prior to implementation at the Servicing Study stage.

Figure 26 - 7.0m Lane Cross-Section



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## Architectural Design

All buildings in Evergreen will be built to conform to the existing City of Red Deer Land Use Bylaw.

Built forms in Evergreen may take their design elements from nature, focusing on visual and physical connections to outdoor spaces. A set of Architectural Guidelines will provide direction toward the overall aesthetic of the community as described on page 66.

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## STORMWATER SERVICING

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## Architectural Design

All buildings in Evergreen will be built ~~to conform to the existing City of Red Deer Land Use Bylaw~~ recognizing the Neighbourhood Planning and Design Standards and Melcor Evergreen Architectural Guidelines.

Built forms in Evergreen may take their design elements from nature, focusing on visual and physical connections to outdoor spaces. A set of ~~Architectural Guidelines~~ Melcor Neighborhood Design Guidelines will provide direction toward the overall aesthetic of the community as described on page ~~66~~ 59.

Pg 51-53

## STORMWATER SERVICING

The City of Red Deer recently completed the installation of a new storm trunk up the escarpment of the Red Deer River Valley. This trunk line, currently being constructed to the intersection of 30th Avenue and The North Highway Connector, will ultimately service a large portion of the future development in east Red Deer. The City is proposing to extend trunk servicing south along 30th Avenue in 2014, in conjunction with the intersection improvements proposed at the intersection of 30th Avenue and 67 Street. The Evergreen development will connect to this new 30th Avenue trunk sewer at the northwest corner of the development. Storm mains will also be designed to convey the controlled release rate from NE 26 while providing temporary pick up of overland drainage from adjacent quarter sections until they are built out.

As previously discussed in this report, there is a large existing wetland feature situated in the middle of the development area. The north portion of this wetland is proposed to be reconstructed in order to act as the primary storm water management facility for the Evergreen development. In order to allow for a conventional City of Red Deer storm pipe system, complete with gravity weeping tile connections to all the homes, the reconstruction will consist of re grading this area to lower the normal water level in the wetland by approximately 2.0m. All storm sewer pipes in Evergreen will connect to this facility, which will provide both water quality enhancement and storm detention, before the storm water outlets into the 30th Avenue trunk system and ultimately into the Red Deer River.

It is proposed that a majority of the south component of the wetland feature be retained in its natural state. This includes maintaining the current normal water elevation in order to protect the existing plant ecology along edge of the wetland. In order to enhance the water

The City of Red Deer ~~recently completed the installation of a new~~ *has an exiting* storm trunk up the escarpment of the Red Deer River Valley. This trunk line, ~~currently being is~~ constructed to the intersection of 30th Avenue and The North Highway Connector, *and* will ultimately service a large portion of the future development in east Red Deer. The City ~~is proposing to~~ *has* extended *this* trunk servicing south along 30th Avenue ~~in 2014,~~ in conjunction with the intersection improvements ~~proposed~~ at the intersection of 30th Avenue and 67 Street. The Evergreen development will connect to this *new* 30th Avenue trunk sewer at the northwest corner of the development. Storm mains will also be designed to convey the controlled release rate from NE 26 while providing temporary pick up of overland drainage from adjacent quarter sections until they are built out.

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It is proposed that a majority of the south component of the wetland feature be retained in its natural state. This includes maintaining the current normal water elevation in order to protect the existing plant ecology along edge of the wetland. In order to enhance the water

quality, it may be necessary to deepen this wetland area though the normal water level elevation will be maintained. To ensure a source of water to recharge this wetland area, some surface runoff from the development will be introduced into the wetland at certain low points in the roadway. These locations are illustrated on **Figure 27 - Stormwater Servicing** on page 61.

Also, to maintain some base flow into the existing ravine, which stretches into the north end of this development, some consideration should be made to installing a small pipe connection from the proposed storm pond facility. This would allow from some water recharge into the ravine but at a very low rate that would prevent downstream erosion which has been an issue in other ravine areas in the City.

All the storm sewer facilities will be designed in accordance with The City of Red Deer Design Guidelines and will be developers responsibility to maintain until the end of the two year maintenance period when this infrastructure will be turned over to The City of Red Deer.

#### **SANITARY SERVICING**

Similar to the stormwater servicing, the City of Red Deer recently completed the installation of a new sanitary trunk up the escarpment of the Red Deer River Valley. This trunk line, currently constructed to the intersection of 30 Avenue and Northland Drive, will ultimately service most of the future development lands in east Red Deer. The City is proposing to extend trunk servicing south, along 30 Avenue, in 2014 in conjunction with the intersection improvements proposed at the intersection of 30<sup>th</sup> Avenue and 67 Street. The Evergreen development will connect to this new 30 Avenue trunk sewer at the

quality, it may be necessary to deepen this wetland area though the normal water level elevation will be maintained. To ensure a source of water to recharge this wetland area, some surface runoff from the development will be introduced into the wetland at certain low points in the roadway. These locations are illustrated on **Figure 27 - Stormwater Servicing** on page ~~61~~ 54.

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#### **SANITARY SERVICING**

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northwest corner of the development. The sanitary sewer system will also designed to convey the design flows generated in NE 26.

All the sanitary sewer facilities will be designed in accordance with The City of Red Deer Design Guidelines and will be the developer's responsibility to maintain until the end of the two year maintenance period when this infrastructure will be turned over to The City of Red Deer. The overall conceptual sanitary sewer system for this development is shown on **Figure 28 - Sanitary Servicing** on page 62.

### **WATER SERVICING**

There is an existing 300mm watermain that is located at the intersection of the 30 Avenue and Carrington Drive. When this watermain is extended north up 30 Avenue in 2015, it will provide water service to the Evergreen development area. There will be a minimum of two connections to this 300mm water line along the west boundary of the neighborhood in order to provide water looping throughout the development. Ultimately there will be an additional looping provided to the future development area to the south. Also as shown there are future water stubs provided to the adjacent land parcel to the east.

### **SHALLOW UTILITIES**

Shallow utility services will be provided by the following companies:

- ATCO Gas (Natural Gas)
- The City of Red Deer E.L. & P. Department (Electricity and Streetlights)
- Telus Communications (Telephone)

sewer at the northwest corner of the development. The sanitary sewer system will also designed to convey the design flows generated in NE 26.

All the sanitary sewer facilities will be designed in accordance with The City of Red Deer Design Guidelines and will be the developer's responsibility to maintain until the end of the two year maintenance period when this infrastructure will be turned over to The City of Red Deer. The overall conceptual sanitary sewer system for this development is shown on **Figure 28 - Sanitary Servicing** on page ~~62~~ 55.

### **WATER SERVICING**

There is an existing 300mm watermain that is located ~~at the intersection of the~~ *in* 30 Avenue ~~and Carrington Drive. When this watermain is extended north up 30 Avenue in 2015, it~~ *that* will provide water service to the Evergreen-development area. There will be a minimum of two-connections to this 300mm water line along the west boundary of the neighborhood in order to provide water looping throughout the development. Ultimately there will be an additional looping provided to the future development area to the south. Also as shown there are future water stubs provided to the adjacent land parcel to the east.

### **SHALLOW UTILITIES**

Shallow utility services will be provided by the following companies:

- ATCO Gas (Natural Gas)
- The City of Red Deer E.L. & P. Department (Electricity and Streetlights)

- Shaw Cable (Cable Television)

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Figure 27 - Stormwater Servicing



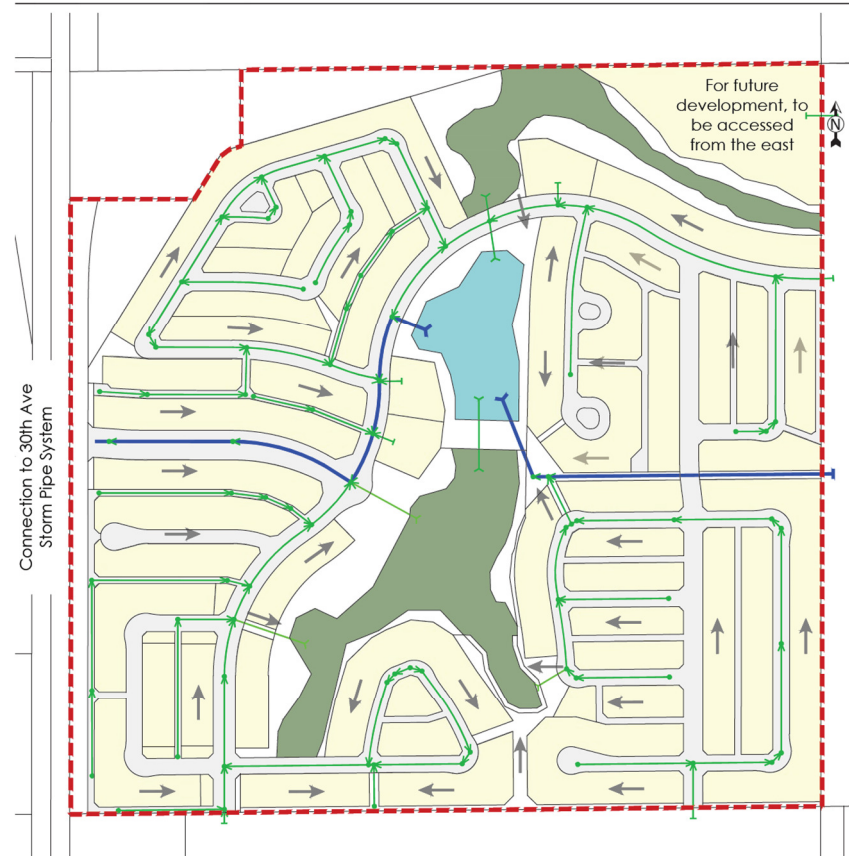
### LEGEND

- |                       |                         |
|-----------------------|-------------------------|
| Storm Water Servicing | Storm Outlet/Inlet      |
| Flow Direction        | Storm Stub              |
| Storm Trunk           | Major Overland Drainage |

- Telus Communications (Telephone)
- ~~Shaw Cable (Cable Television)~~ Rogers (Cable Television)

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Figure 27 - Stormwater Servicing



### LEGEND

- |                       |                         |
|-----------------------|-------------------------|
| Storm Water Servicing | Storm Outlet/Inlet      |
| Flow Direction        | Storm Stub              |
| Storm Trunk           | Major Overland Drainage |

## Page 62

Figure 28 - Sanitary Servicing



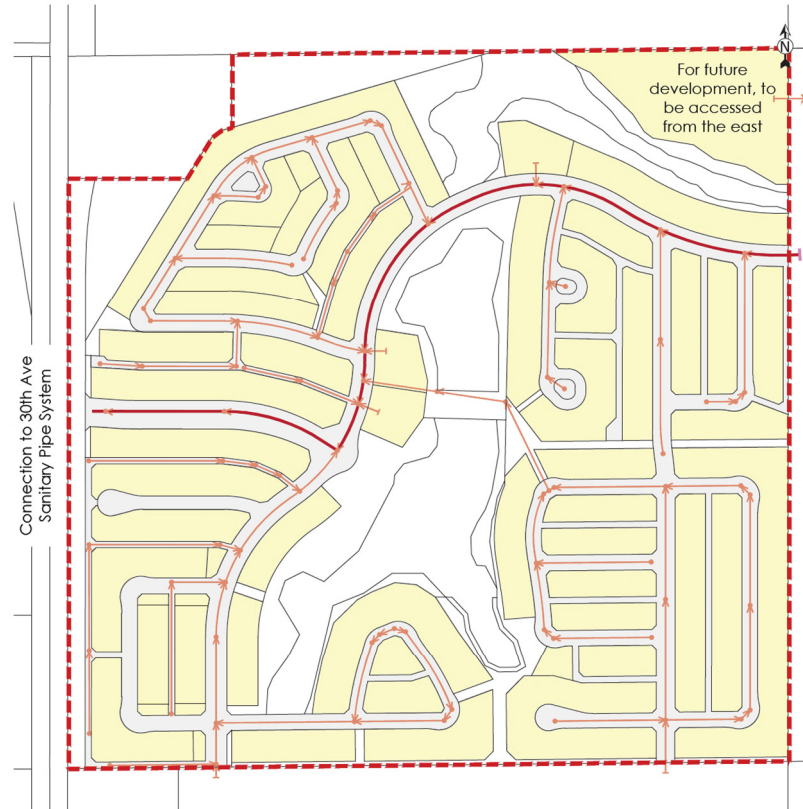
### LEGEND

- |  |  |   |
|--|--|---|
|  Sanitary Servicing |  Sanitary Trunk   |  Sanitary Stub |
|  Flow Direction     |  Manhole Location |   |

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## Page 55

Figure 28 - Sanitary Servicing



### LEGEND

- |  |  |   |
|--|--|---|
|  Sanitary Servicing |  Sanitary Trunk   |  Sanitary Stub |
|  Flow Direction     |  Manhole Location |   |

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Figure 29 - Water Servicing



Figure 29 - Water Servicing

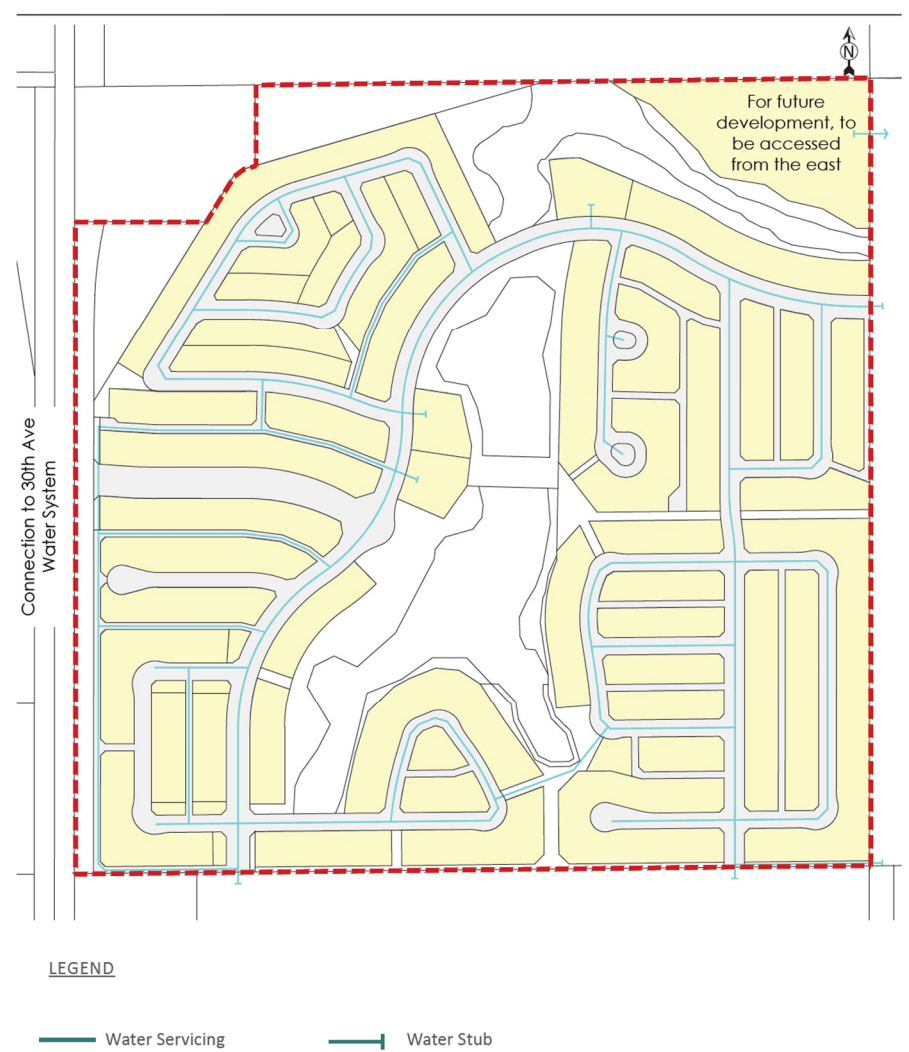




Figure 30 - Phasing Plan



LEGEND

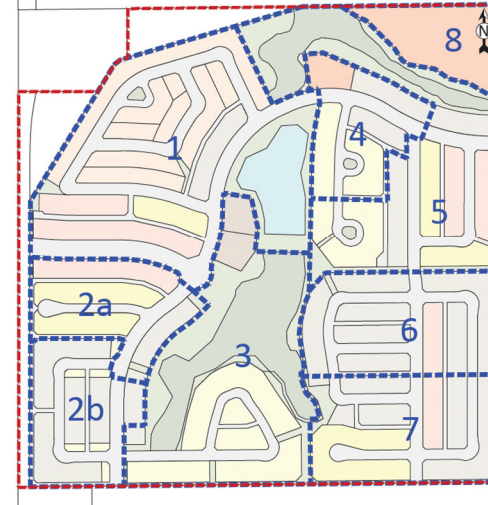
- Phasing Boundary
- Phasing Sequence

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## REDESIGNATION AND SUBDIVISION

Redistricting and subdivision applications, to conform to the land use designations described in this NASP, will be undertaken as necessary. Guided by The City of Red Deer Municipal Development Plan, The City of Red Deer East Hill Major Area Structure Plan, and the Evergreen NASP, redesignation and subdivisions must conform to The City of Red Deer Land Use Bylaw and all applicable statutory plans in addition to the informational requirements necessary for each application.

Figure 30 - Phasing Plan



LEGEND

- Phasing Boundary
- Phasing Sequence

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## REDESIGNATION AND SUBDIVISION

Redistricting and subdivision applications, to conform to the land use designations described in this NASP, will be undertaken as necessary. Guided by The City of Red Deer Municipal Development Plan, The City of Red Deer East Hill Major Area Structure Plan, and the Evergreen NASP, redesignation and subdivisions must conform to The City of Red Deer ~~Land Use~~ **Zoning** Bylaw and all applicable statutory plans in addition to the informational requirements necessary for each application.